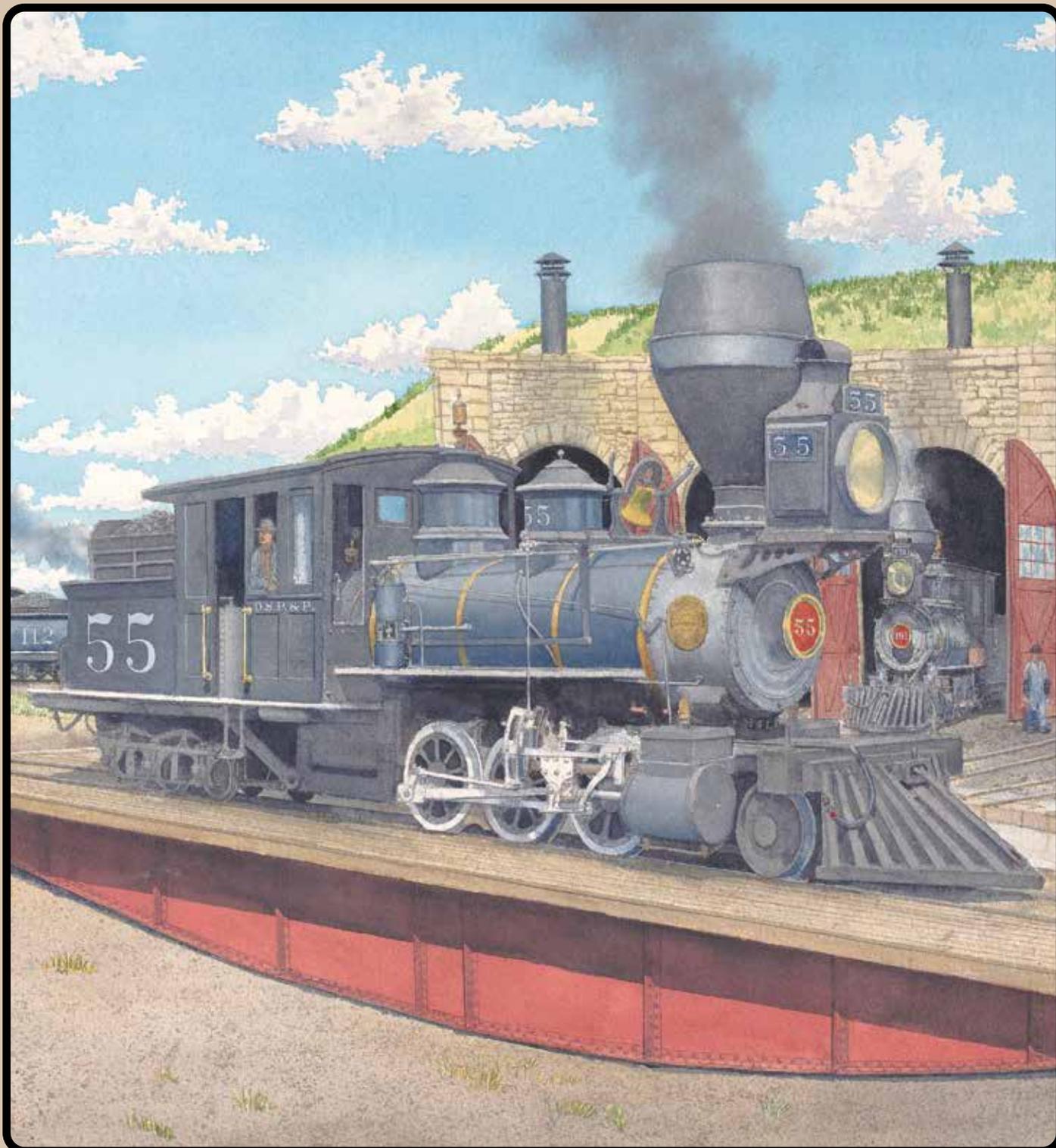


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MARCH/APRIL 2022

VOLUME 48, NUMBER 1



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COVER

John Coker painted this issue's cover. It depicts Denver South Park & Pacific #55 being turned on the Como, Colorado turntable. John sells his art and accepts commissions at john@cokerart.com.



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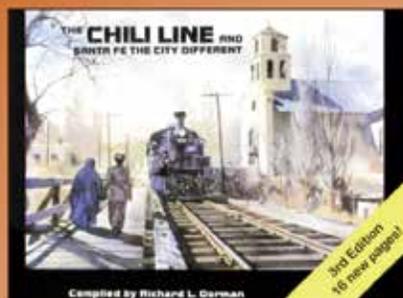
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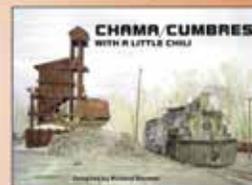
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Robert's Ramblings

WHAT TO DO?

Photos by the author

Editing Joe Crea's Pitkin Sand & Gravel article on page 22, brought back memories and may have pushed me into doing something. One of Joe's quarry locomotives was converted from a Regner Diesel. Joe converted his model to 1/2n2 and described it in the September/October 1993 *GAZETTE*.

About that time, Regner sent me a train set for review. It consisted of an 0-4-0 Diesel (Joe's model), four gondolas, and a circle of snap track. If you are not familiar with Regner, it is a German company producing five different 1/2-inch scale industrial Diesels including a center cab, and two open cab models. There are also disconnected log trucks, high and low side gondolas, two little flat cars with stakes, and a small animal (stock) car. The models run on 30mm gauge track. They are designed to run as industrial feeder lines on LGB layouts. Flextrack and turnouts are available, and at one time, Regner made dual gauge track and turnouts for running with LGB track. Today, Regner RSSB also lists seven kits for small scale steam locomotives with adjustable

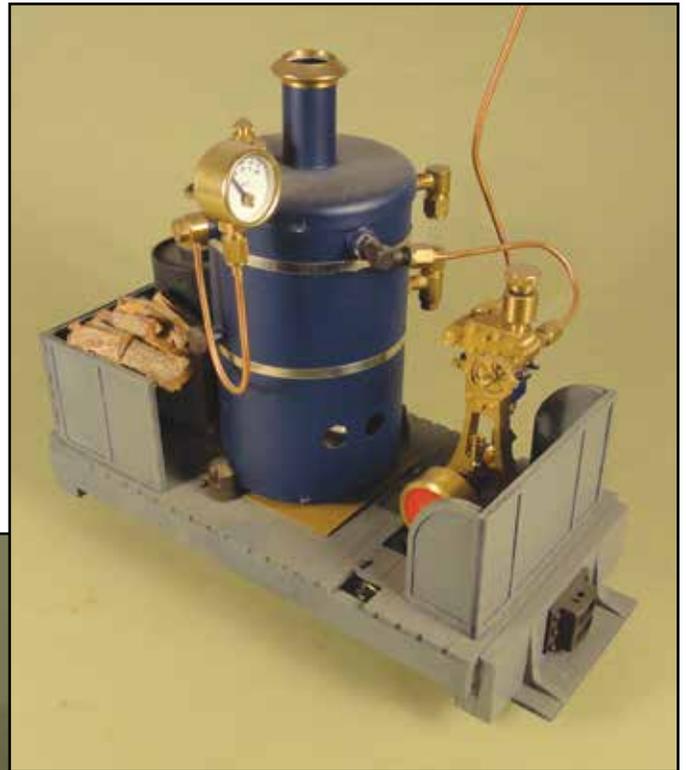
Below: This Regner Diesel and two gondolas were sent for review. That's one of the left hand turnouts in the foreground.



gauges. I have assembled a vertical boilered Loki and had a lot of fun doing it.

Over the years, Regner sent examples of their rolling stock, but no track. I kept taking the models out and wondering what I could do with them. They are plastic with LGB type loop and hook couplers and oversize flanges. But as Joe showed me, they could be converted into nice scale models.

But before I could do anything, I needed track and turnouts. I just wasn't in the mood to hand lay track, and my attempts to convert pieces of the snap track into turnouts were a disaster. So, starting with Regner, I contacted hobby shops around the world. Finally, I found a shop in England with 30 feet of flextrack, so I bought it. I could find no turnouts, so I again put everything away. A year later, the English shop emailed to say he had four left hand Regner turnouts and did I want them? I opted to buy two turnouts giving me a Diesel, cars, a partial circle of snap track, 30-feet of flextrack and two left hand turnouts. What to do?



Above: This little Regner vertical boilered live steam locomotive was fun to assemble.

Left: My 1/2-inch scale gas station needs signs, and details.



I sketched out a little switching line with an engine shed, crossroads with a rural gas station, and some sort of sand and gravel transfer. Maybe a river barge? I even built the 1/2-inch scale gas station.

But I really did not have room for my switching layout and put everything away again.

So maybe Joe's quarry line will help me decide what to do and I will design, and maybe build, a line with two left hand turnouts.

Bob Brown

Oops...

The author of *The Gilpin Tram in On30*, *Senior Citizens Dream* on page 58 of the January/February GAZETTE is Jim Murphy, not Tim as stated. My apologies Jim. *Bob Brown.*

Last Run...

While I do not have full details, I feel GAZETTE readers should know that **Bruce Kinker** of Southwest Narrow Gauge, and **Stan Cedarleaf** of Cedarleaf Custom Decals, have passed away. Our condolences to their families. *Bob Brown.*



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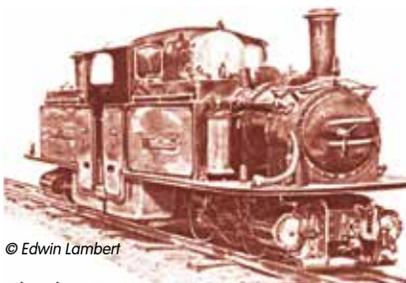


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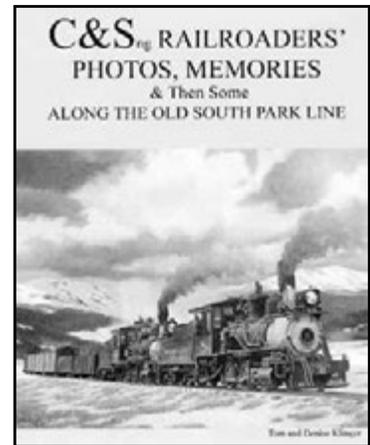
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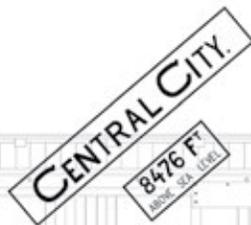
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The Pigeon Hole

Dear Bob,

I recently visited Roaring Camp to inspect the Jim Vail Memorial bench. It is in good shape and in a great location for train watching.

Sincerely,
Jim Blain
Via email



Editor's Note: Letters chosen for publication in "The Pigeon Hole" may be edited for length and clarity.

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Dear Bob,

When I read Craig Symington's article on HO_{n3} kits in the November/December 2021 *GAZETTE* I was specifically interested in his comments about Rail Line boxcars. I too believe these cars are some of the nicest cars to build. However, the metal roof is not often found on western coast narrow gauge lines. So, I undertook a little kit bashing and added scale styrene 2 x 6 pieces between the cast roof boards, removed the end roof walks, and made my car a little sway backed, by cutting or breaking the thin plastic under the side door frames. Then I painted and weathered my car giving it a unique western look.

Sincerely,
Steve Bradley
Via email

Dear Bob,

Interesting comment by John Nichols on researching the real configuration of the historic Pescadero Creek RR on page 12 in the January/February 2022 *GAZETTE*. The Railroad Valuation Records may have just what he is looking for, Google RR Valuation Records at the Library of Congress. Also, the Lands Office of the Bureau of Land Management often has records not found elsewhere. I learned about these sources when doing archaeological research on the RRs of Rocky Mountain National Park and other areas of Colorado.

Sincerely,
Bill Butler
Via email

Dear Bob,

Regarding Johnny Graybeal's excellent series on ET&WNC locomotives and the #12 model by Bachmann. I have the following information by the late Jim Hobbs. The Bachmann pilot model was a kitbash created by Jim Hobbs, retired Vice President-Operations of the Richmond, Fredericksburg & Potomac Railroad for his own model railroad. Lee Riley of Bachmann "borrowed" the kitbash to create the Bachmann Big Hauler. Jim told me that Lee provided him with one of the first run of that model because his kitbash had gone to China and Jim suspected it had been disassembled when the Chinese factory reverse engineered the kitbash to create the Bachmann production model.

The reason the Big Hauler was spot on in most of the important measurements was due to Jim being an excellent craftsman, together with his background for information to create the model. Jim's father was Chief Mechanical Officer for the ET&WNC, and Jim told me many a tale of accompanying his father to work during Jim's formative years. Jim was an active member of the East Tennessee & Western North Carolina Railroad Historical Society. All that gave Jim the details and drive to create that excellent model.

Jim was an avid scale outdoor railroader, running 70+ reefer blocks with double headed WP&Y Diesels on over ¼ mile of backyard track on his Pine Tag & Gumball Railroad.

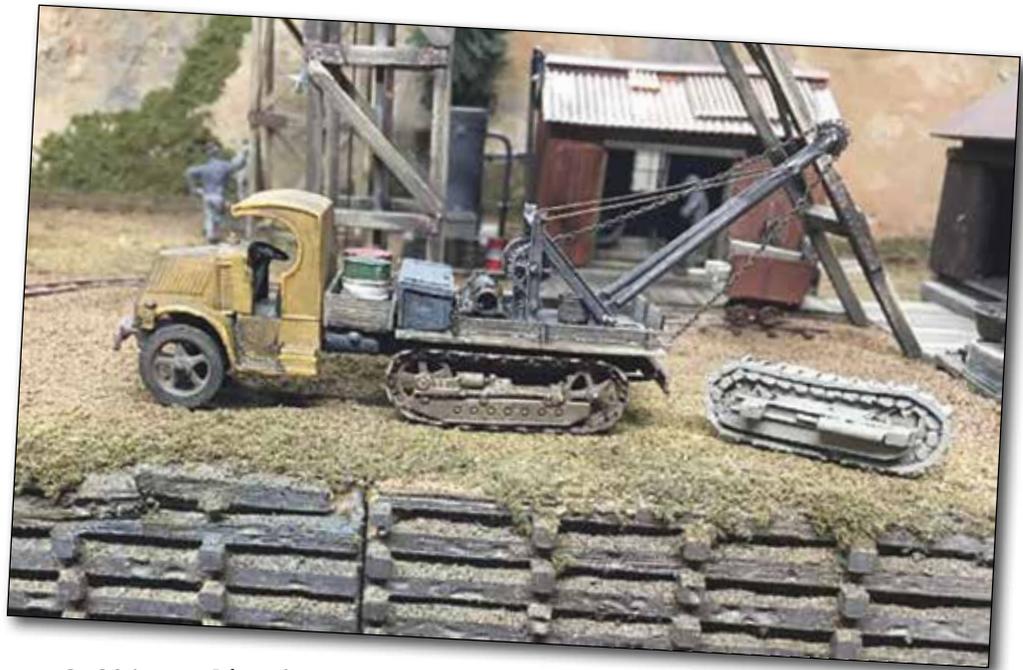
Sadly, Jim passed away on May 25, 2021, leaving many a tale yet to be told.

Sincerely,
Ken Montero
Via mail

Dear Bob,

During many summers in my youth, I worked for Rice Mill Wright Service to make money for school. The company did mostly on the spot repairs or replacement of machinery for cattle feed lots, flour mills, and large ranches. One such place was DM&H Cattle in Henderson, Colorado, 35 miles north of Denver, where there were two surplus Army halftracks. One had a small crane on the rear section of the chassis over the tracks. The image of this vehicle stuck with me until I finally had to make one for my On30 layout. I found some track bogie castings in my parts drawer, along with a Match Box Mac C cab truck. With the addition of a Berkshire Valley winch kit, my halftrack came to life.

Sincerely,
Alan Olson
Via email



Dear Bob,

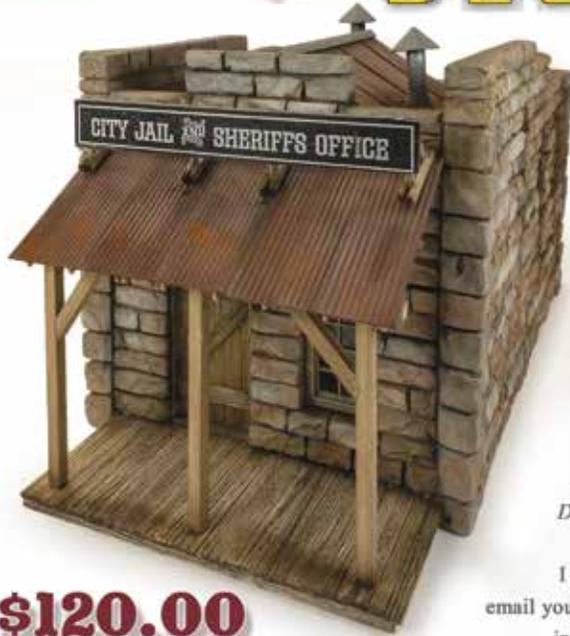
I thoroughly enjoyed Craig Symington's HOn3 article in the November/December issue. I was particularly pleased to see his tribute to our late friend Keith Koch. I worked with Keith for more than ten-years restoring cars at the Ridgway Railroad Museum. When we had finished D&RGW gondola #703 we argued whether to use the so-called toilet seat logo of the 1930s, or the Flying Grande loco of the 1940s. We couldn't agree, so we lettered one side with the toilet seat logo

and the other with the Flying Grande logo, I also have one of Keith's San Juan Swill cars and have included a photo of it at the San Juan Swill Brewery in Montrose on my HO/HOn3 railroad.

Sincerely,
Don Paulson
Via email



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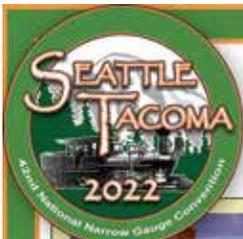
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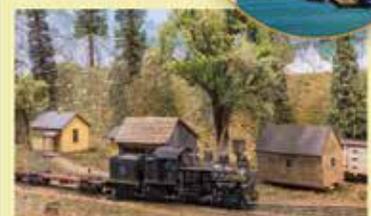
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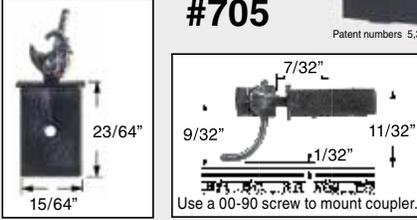
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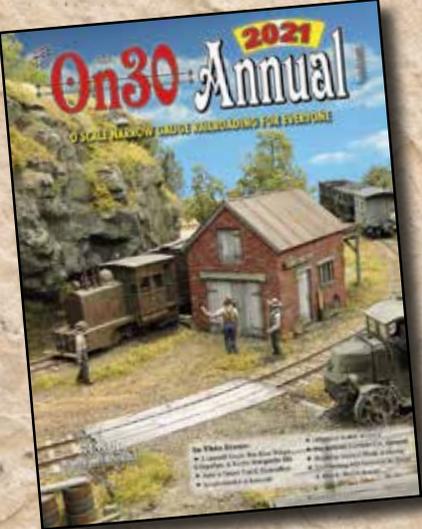
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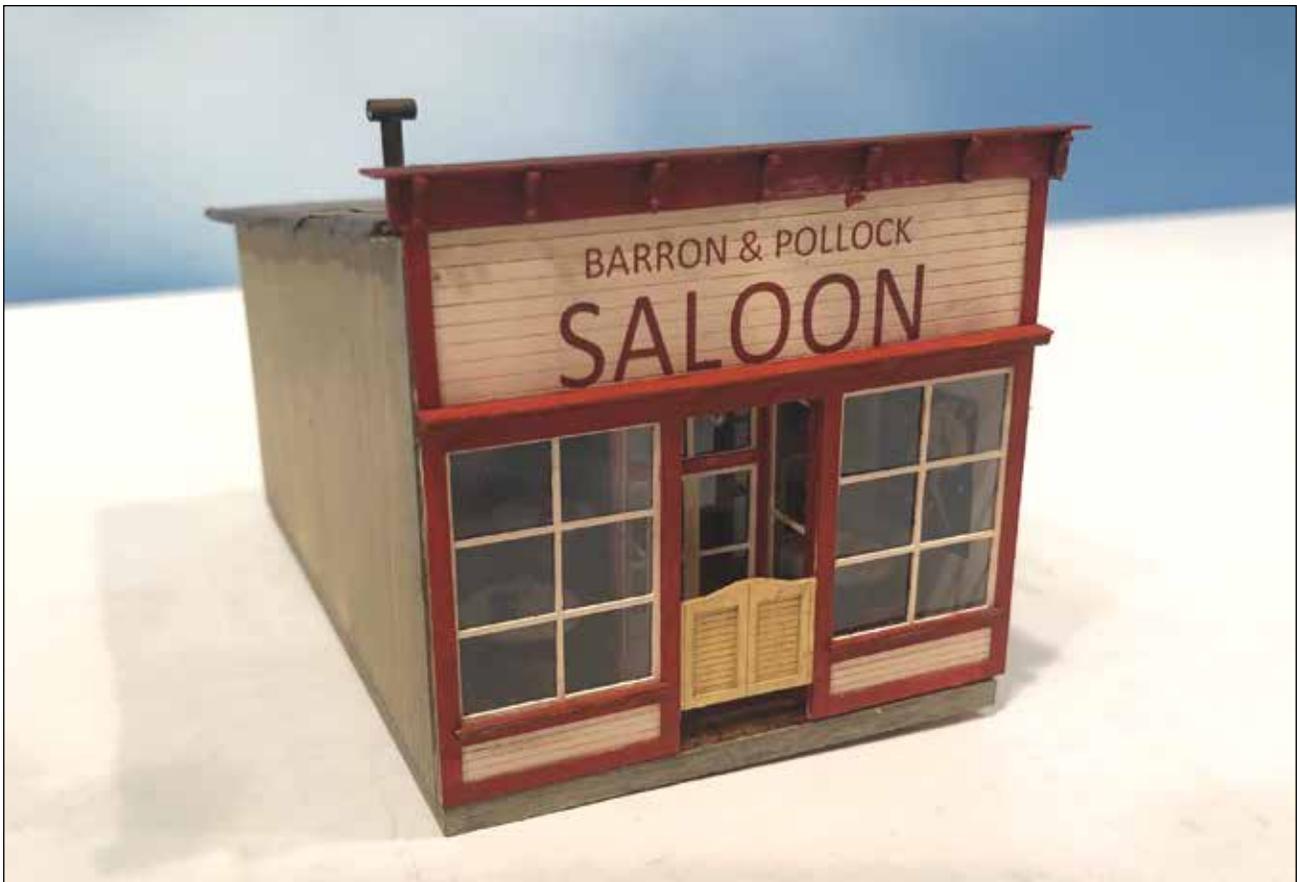
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New in Review

Berkshire Valley Models, 438 Morgan Woods Dr., Fenton, MO 63026, www.berkshirevalleymodels.com has released another in its HO scale false front building in their western series, #2034, the Saloon for \$27.95. As with previous releases, this one is composed of primarily laser-cut wood and laser-board components, acetate, white metal castings, decals for the Barron & Pollock Saloon and full instructions. As with previous kits, this one is well engineered, and everything fit perfectly. The large windows call for an interior and I added one, partly created from castings from my scrap box along with some figures and partly from a saloon graphic found on the internet. An interior wall and door are included but because of the graphic that extends across the back, I saved those for another project. I added one light for the interior and another over the recessed front door. The engineering allows the building to be press-fitted upon the foundation and/or the roof to be made removeable. I opted for removal of the building from the foundation to allow the interior light to be mounted on the ceiling.

Assembly was straightforward and well covered by the instructions. I was able to complete assembly, even with an interior, in two sessions. The windows/doors are from laser-board and assemble easily. A couple of notes. Watch the amount of tarpaper used, as just enough is provided for the job. Judicious splicing will do the trick. Paint as you go and take advantage of the layered construction to use different colors for a clean delineation. I used craft paints with a buff-colored front, iron oxide red trim and a neutral wood color for the side/back walls. Often only the front wall received fancy paint and gingerbread trim. One last note: The Barron & Pollock Saloon is named after the late Jim Barron, and Allen Pollock, former NMRA President, renowned narrow-gauger and owner of Missouri Locomotive Co., who is still very much with us. Both are known for the occasional drink. By any measure, BV is to be commended for a very nice kit, but also for a fun christening. I enjoyed building the Barron & Pollock Saloon and look forward to new releases in the series. *Charlie Getz.*





Crow River Products, P.O. Box 2092, Pawtucket, RI 02851, www.crowriverproducts.com, sells an extensive line of detail parts and industrial machines in HO and O scale. When I mentioned in the January/February Robert's Ramblings that I needed a boiler for my Eureka Mill stamp mill, *GAZETTE* author, Lind Wickersham, emailed to remind me of the boiler kit sold by Crow River Products. So, I checked their web site and ordered a kit for a Return Flue Industrial Boiler. It sells for \$95.00, and I received my kit in just four days. The kit consists of cast resin and white metal parts, strip wood, aluminum tubing, wire, a styrene tube for the smokestack, and printed steam gauge fronts. The brick enclosure is a one-piece resin casting with excellent brick detail. The boiler top and front are also resin castings and have very little flash. The details such as a flue tube sheet, fire doors, steam dome, valves, faucets, pipe flanges, sight glass, and various braces are cast in white metal. There are four pages of instructions, and another four pages of drawings showing where all the details go.

I used Model Master Honduras Maroon (I had it on hand) to spray the bricks. Then after the bricks dried, spotted some Microlux white on them and quickly wiped the white away with a cloth dipped in water. The boiler parts were sprayed Testors Black, and I brushed the details with Floquil Engine Black. When I was through assembling the kit, I dusted it with rust and black powders to blend it all together. I am pleased with my boiler and now must make sure that tall smokestack fits through my roof in the right place in my mill. *Bob Brown.*



Bollinger Edgerly Scale Trains, 375 Bean Hill Rd., Belmont, NH 03220, 603/267-1818, www.besttrains.com sells alcohol-based stains called Vetero Solutions. They come in 6.7-ounce bottles for \$14.95 a bottle and can only be shipped within the United States. *BEST* lists 18 colors on their web site along with several videos of modelers using them. The stains come in colors such as Caribbean Pink, Glacier Bay, Fog, and Light Rust. I chose to buy a bottle of Aged Barn Wood, and Murky Brown. Both stains worked perfectly on my basswood sample and the color can be built up with additional applications. The stain can be used on many materials such as wood, paper, cardstock, plastic, plaster and hydrocal. It cleans up with soap and water, or rubbing alcohol, has no odor, and each bottle comes in a nice cardboard box great for storing stuff. *Bob Brown.*



Evergreen Scale Models, 65 East Bradrock Drive, Des Plaines, IL 60018 sells, in addition to their extensive line of sheet and shaped styrene, CA adhesives, and styrene adhesive, but also Yellow Carpenter's Glue, White Glue and Canopy Glue. The Carpenter's Glue, White Glue, and Canopy Glue are available in 2-ounce bottles for \$4.15 or in 4-ounce bottles for \$5.56. *Bob Brown.*



Inter-Action Hobbies, P.O. Box 1021, Chase, British Columbia, V0E 1M0, Canada, 1-888-850-1742, www.interactionhobbies.com, offers the HO scale Pork Store for \$58.95 (U.S.) with optional interior details/lights for \$24.95.

If the name Satriales and the look seem vaguely familiar, it is because this kit is based upon the fictional New Jersey store/restaurant of the same name featured in the HBO TV series, *The Sopranos*. Though I was not a viewer of this series, I believe this was the store owned by the mob, won in a card game. It was not the site where Tony got whacked; that was a fictional diner, but no doubt violent things happened here as well. Fortunately, nothing violent happened as I assembled this highly detailed and well-engineered kit. All the parts fit perfectly, though the instructions were a bit off on some parts identifications, an error owner Darryl Jacobs notes will be corrected. Wall "12" is really wall "21," for example, and the rear canopy roof is mis-identified. The rest sort themselves out, however. Study the instructions and pictures so you can tell what to do if not covered by the instructions, such as where to place the interior restaurant divider wall graphics (toward the front). Again, no problem, just pay attention.

The interior sets this kit apart and is worth the extra investment. 3D printed parts, an LED light bar, and additional laser-cut parts allow both the meat market and restaurant sections to be fully detailed. There is a fair amount of very demanding detail assembly/painting involved. The instruction photos show every can with a full color label, and every detail is similarly decorated. By the way, Part 11

is located with this detail set though the instructions refer to it as if it's part of the main kit. The fit of the detail parts is exact. Work slowly, making sure you understand what goes where. The light bar requires soldering, and mine is mounted on a craft stick glued to a notch in the walls so the removeable roof does not interfere. I was intrigued by the two layer checked tablecloths; a decal placed on tissue paper and draped over the laser-cut table with copious amounts of white glue. Very effective, though fussy. I also created a jig to ease the assembly of the chairs. I even added a wood plank floor made from Paper Creek wood siding (out of production) and some table-top items from my scrap box. A few figures complete the scene. By the way, I could not resist modifying a seated figure and adding some red paint to represent a customer just after getting whacked. The rest of the customers and beat cop don't even notice. Macabre but different.

Though the instructions suggest this store represents a recent prototype, even with the aluminum storefront windows/doors, it could date from the late 1940s, the era I model. In that era, often older storefronts were modernized with a sleeker new aluminum storefront. I left off the contemporary satellite dish antenna, parking bumpers and trash dumpster, and painted the rear windows/doors a more traditional color. I also substituted scratchbuilt roof Globe ventilators for the very nice, more modern ventilators provided. I added a few kitchen stacks and garbage cans as well. And I used my favorite CC Crow seamed tarpaper rather than the recommended tar/gravel roofing. I also took the lovely 3D printed pay phone detail and added

it to an older kit for a wooden phone booth. These steps backdated the building sufficiently.

The exterior signage adds a lot also. A provided double-sided hanging sign can be illuminated by adding a bulb or LED into the sign box with wiring routed through the aluminum tube/hanger provided. I did so, though I modified the box by making it a bit thicker for the light and by using a slightly larger aluminum tube for the wiring. Satriales

will add a different look to your Main Street and surely elicit comments from visitors when lit, though perhaps not the ones you intended if they spot the whacked customer. The interior is stunning with the counters, freezers, and level of detail. I now need to add one of Inter-Action's new cigarette machines to the provided front sidewalk. *Charlie Getz.*



Sn3 model

West Side Lumber Co. Logging Flat Car

For most of the last century, the WSLCo hauled huge logs from the California forests on simple, small flat cars. The total number of these cars constructed was in excess of 300 units. This kit can be built as the standard logging flat car, or use the additional parts included to build the equipment flat car as shown above.

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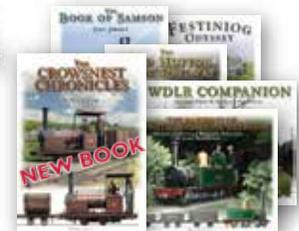
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PITKIN SAND & GRAVEL



SOME BACKGROUND

Colorado is perhaps best known for its mountainous terrain. The San Luis Valley in south central Colorado is the setting of my most recent model railroad. This high mountain valley is bounded on the east by the Sangre de Cristo Mountain range and on the west by the San Juan range. The valley floor is relatively flat and sits at an elevation of over 7000 feet. The Rio Grande River flows through the valley. Covering an area of 150 miles in length and about 50 miles wide, the valley boasts some towns with which you may be familiar: Alamosa, Antonito and Creede. But other lesser-known small

A Different Kind Of Colorado Railroad

by Joseph Crea
Photos by the author

Title photo: An overall view of the Pitkin 1/2-inch scale, Sand & Gravel quarry, showing the operating drag-line, a portable air compressor, and the Caterpillar tractor.

towns also exist in this area: Monte Vista, Del Norte, Saguache and South Fork.

It is this valley that I chose as a locale for my new model railroad. To utilize equipment and rolling stock from past efforts, I envisioned this railroad as a small 30-inch gauge quarry railway, whose purpose was to gather sand and gravel from the quarry and deliver it to a standard gauge connecting line. I wanted to “get a feel” for the topography of this area, so my wife and I drove there from north Denver. We tied up for the night at a motel in Del Norte. At about 11 pm, we were startled by a thunderously loud Diesel locomotive passing nearly under our window, its powerful engine

reverberating and pulsing off the motel's brick wall. As we discovered later, this train makes its way up the valley at night to avoid automobile traffic at the grade crossings. It visits various small industries and locations in the valley, delivering empties and pulling out loads. This is the San Luis & Rio Grande Railroad, and is the last remaining vestige of the D&RGW in the valley. I now had the perfect prototype to serve as the standard gauge connection to my narrow gauge quarry railway. As we drove down the valley, I stopped frequently to take photos, including a series of landscapes, I hoped would serve as a geographically correct backdrop for my railroad. In addition, I discovered to my surprise that there is an old, abandoned quarry near Del Norte, although it produced cut building stone, rather than sand and gravel, and never had a railway.

THE LAYOUT

Inspired by a Tom Yorke track plan, I designed my layout to fit in a confined area of my home, so it is of modest size, despite being 1/2-inch scale. Consisting of a quarry and track leading to a standard gauge interchange with a small engine terminal, I wanted something I could have up and running quickly. The quarry itself was the most challenging and interesting segment to construct. I based the design of my small quarry on several quarries I had researched, as well as the Tom Yorke

track plan, featuring track that winds tightly down to the floor of the quarry, where tipper cars can be loaded. I used the cookie cutter technique with 3/8-inch plywood to construct the basic sub-roadbed here. After installing sufficient risers to support the plywood, I installed O gauge flextrack. O gauge track becomes 30-inch gauge in 1/2-inch scale. The curves and grades here are outrageously sharp and steep, but this is an industrial railroad with tiny equipment. This approach mandates that all my turnouts be scratchbuilt since standard O gauge turnouts are much too long and shallow. Simple Caboose Industries ground throws were utilized on all turnouts, in keeping with the industrial nature of the layout. These throws look quite good in the larger scale after a bit of weathering. Following completion of the track and wiring, I inserted random-thickness chunks of pink foam between the layers of plywood sub-roadbed to form the basic walls of the quarry, securing them in place with construction adhesive. Spackling was used to fill any gaps between the foam pieces. Then I applied a thick coat of earth-colored latex paint, and literally threw sand onto it while wet. Additional small rocks were added later and adhered with white glue.

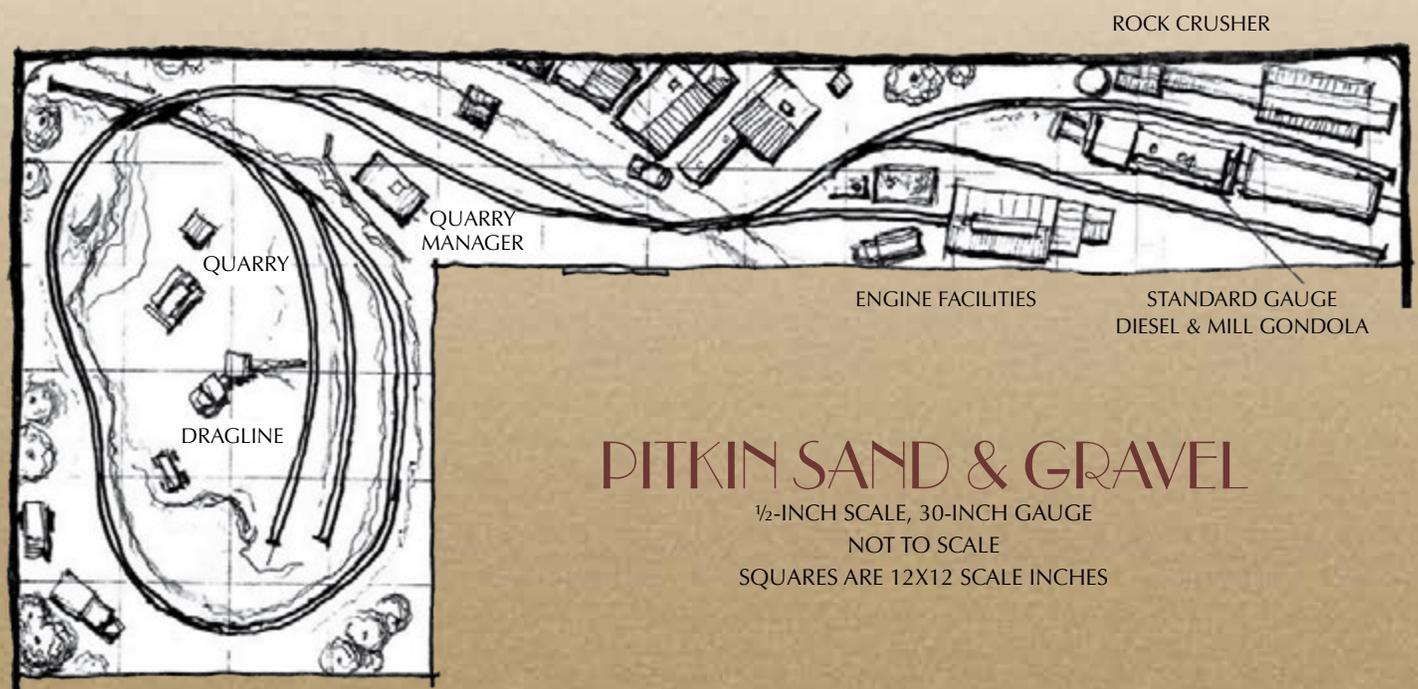
The remainder of the layout is constructed with L-girder framing, topped with plywood or pink foam. I found a local source for suitably sized gravel and earth ground cover, and this was adhered with white glue.

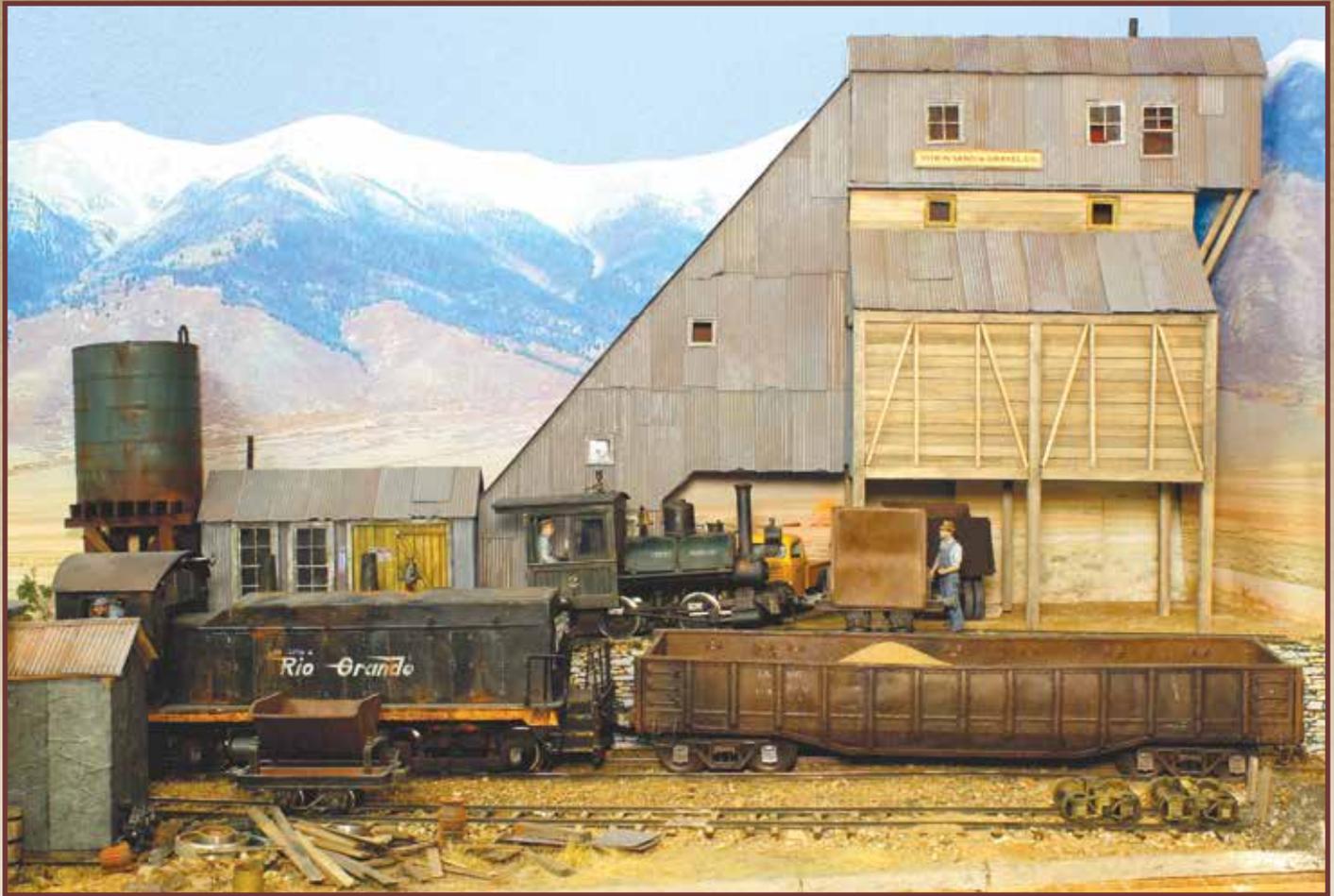
LOCOMOTIVES & ROLLING STOCK

Since 1/2-inch scale, 30-inch gauge is not a common scale/gauge combination, I had to scratchbuild or kit bash most of my locomotives and rolling stock. The locomotives include a modified O scale Diesel by Atlas, a European Regner industrial Diesel, and a Brookville industrial locomotive from Tom Yorke. I constructed a new larger cab for the O scale locomotive and added various 1/2-inch scale detail parts. Similarly, I modified the Regner locomotive to have a more American appearance and re-gauged it. In addition, I scratchbuilt a freelanced 0-4-0T tank steam locomotive, designed to resemble a Porter product. I use 1:29 scale equipment to represent standard gauge, even though the scale and gauge are not exactly accurate. I enjoy seeing the contrast in size between the large standard gauge equipment and my tiny narrow gauge gas mechanicals and rolling stock. I also scratchbuilt a string of 4-wheel tipper cars from styrene and ABS for use in the quarry.

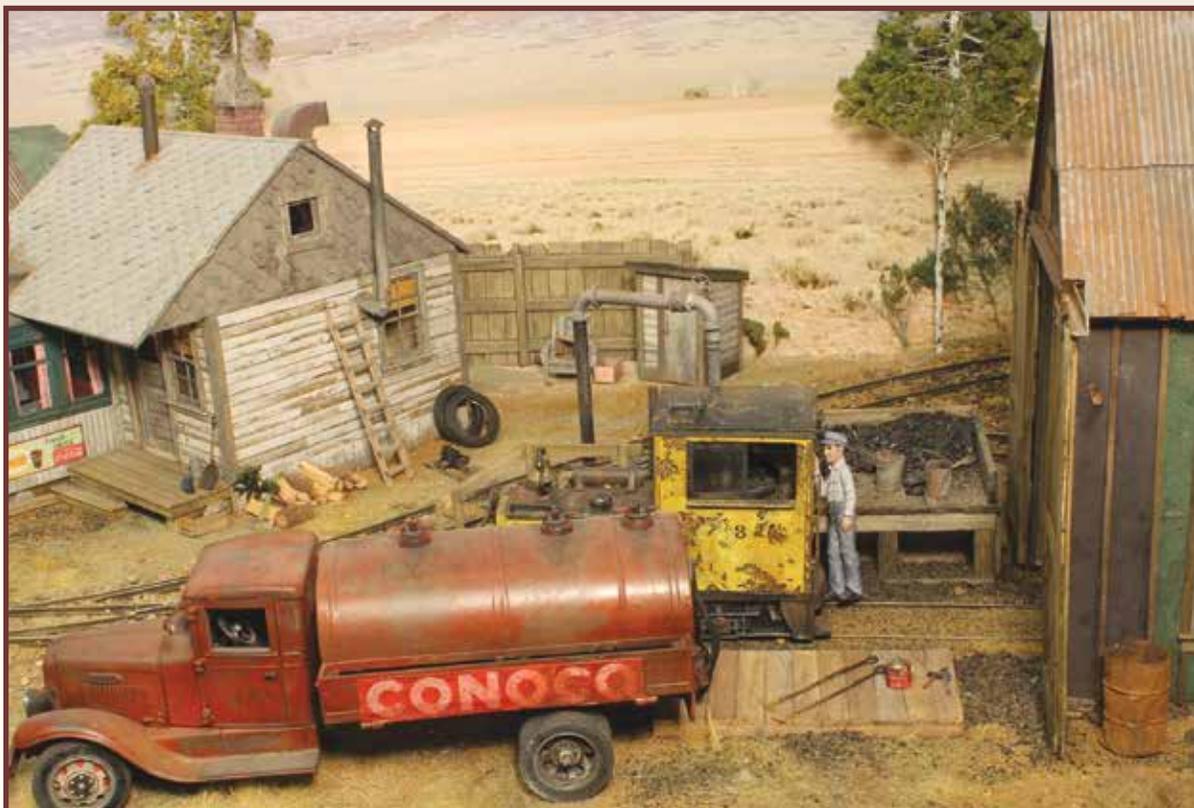
I acquired a USA Trains EMD NW-2 Diesel locomotive and a mill gondola to serve as the standard gauge connection. These were both detailed, re-painted and weathered to replicate well-used and tired equipment. I lettered the locomotive for the San Luis & Rio Grande RR. There was never any intent to operate these items, although I do have operational lights on the locomotive and a sound chip

(text continued on page 26)





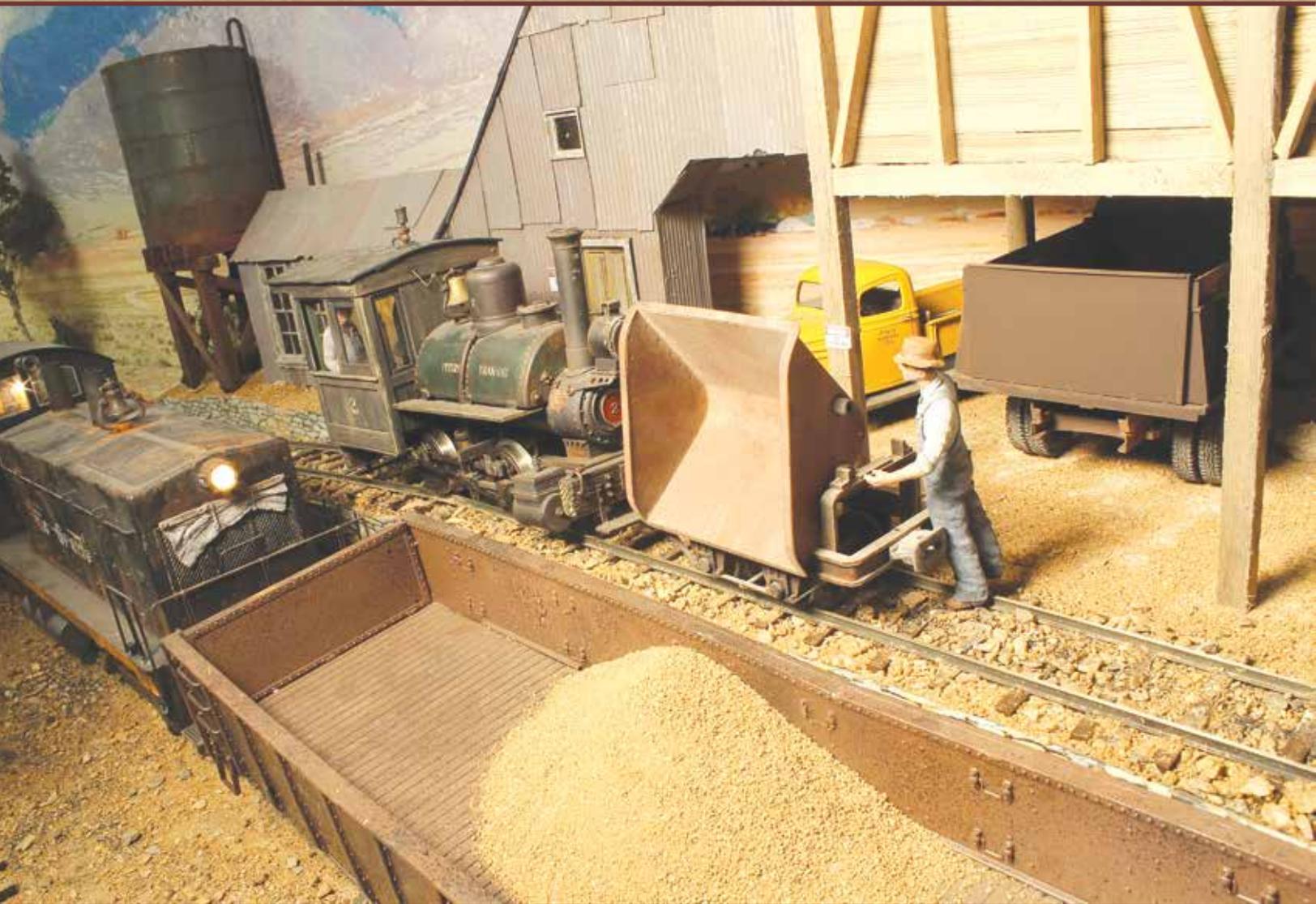
Above: This free-lanced rock crusher towers over the standard gauge interchange siding, where the NW-2 and mill gondola wait to be loaded.



Left: Gas mechanical #3 pauses to have its fuel tank refilled from the old CONOCO tanker truck used by the railroad. Note the clutter, and weathering making the quarry and its surroundings look well used.

Opposite: Locomotive #2, the only steamer on the small railroad, simmers inside the engine shed in the early morning sunshine.





Above: Sand is dumped from a 30-inch gauge tipper car into the standard gauge mill gondola. Locomotive #2 is scratchbuilt mostly from styrene, with added weight.

(text continued from page 23)

of an idling Diesel. They sit on a siding, waiting for the gondola to be filled from the quarry tipper cars.

STRUCTURES & VEHICLES

All structures are scratchbuilt, except for a die cast camping trailer which serves as the home and office of the quarry manager. I weathered and detailed this item, fitting a TV antenna, corrugated skirting under the trailer body, an added wood porch, a clothesline, pink flamingoes, and scratchbuilt camp chair.

There is a small village on the layout, with only a few structures. The grocery store is based on a Randsville,

California, prototype and the derelict Jefferson Dry Goods store is the second iteration of this free-lanced building. The original was my first large scale structure, scratchbuilt some thirty-five years ago. This replica is a very limited edition lasered kit from Phil Dippel. The diner is an interesting structure based on a unique prototype building (although not a diner) that exists a few miles from my home. I have been unable to determine the origins of this wonderful little gem, but it consists of a vintage wood railroad coach attached to a small cottage. I decided to turn it into a diner with the assistance of a Bachmann coach body. Interior lighting and complete interior detailing make it an attractive feature on the layout. Lighting in my buildings was done with LED units from Evan Designs.

Other scratchbuilt structures include an engine shed and rock crusher. The engine shed is impossibly short, but then, so are my locomotives. An abbreviated version of a C&S coaling platform and water plug based on a Leadville, Colorado, prototype, serve to fuel the steam locomotive. An old fuel tanker truck provides fuel for the gas mechanical locomotives. The rock crusher building was cobbled together with a basic structure of Task Board overlaid with paper corrugated metal sheathing.

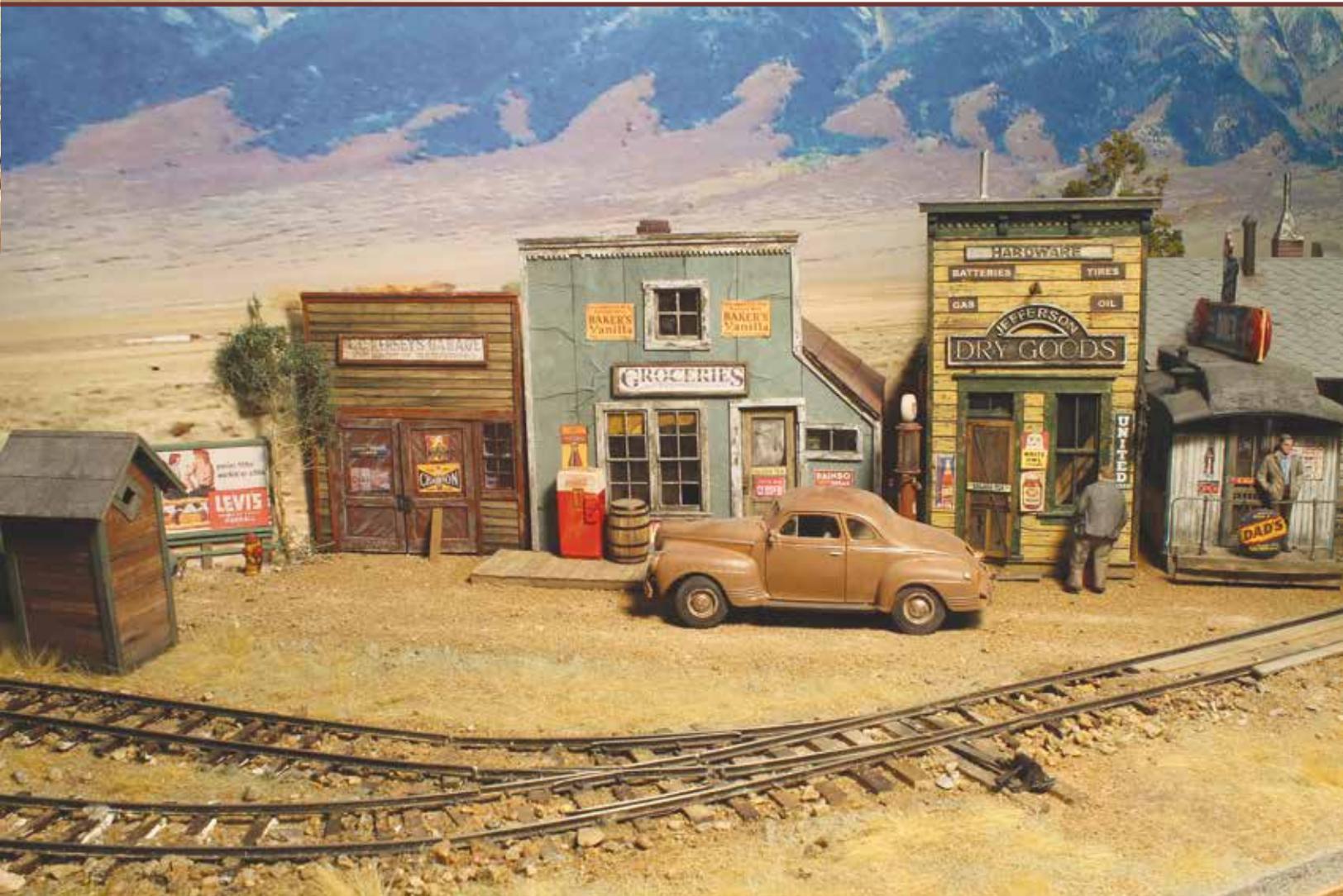
Various plastic kit and die cast vehicles were used on the layout, all weathered and modified in some way. I enjoy adding license plates for my vehicles, correct for the period.

(text continued on page 28)



Left: Scratchbuilt locomotive #2 steams past the quarry manager's trailer office, with its pink flamingoes and canvas lawn chair. The porch was patterned after a prototype in the small mountain town of Pitkin, Colorado.

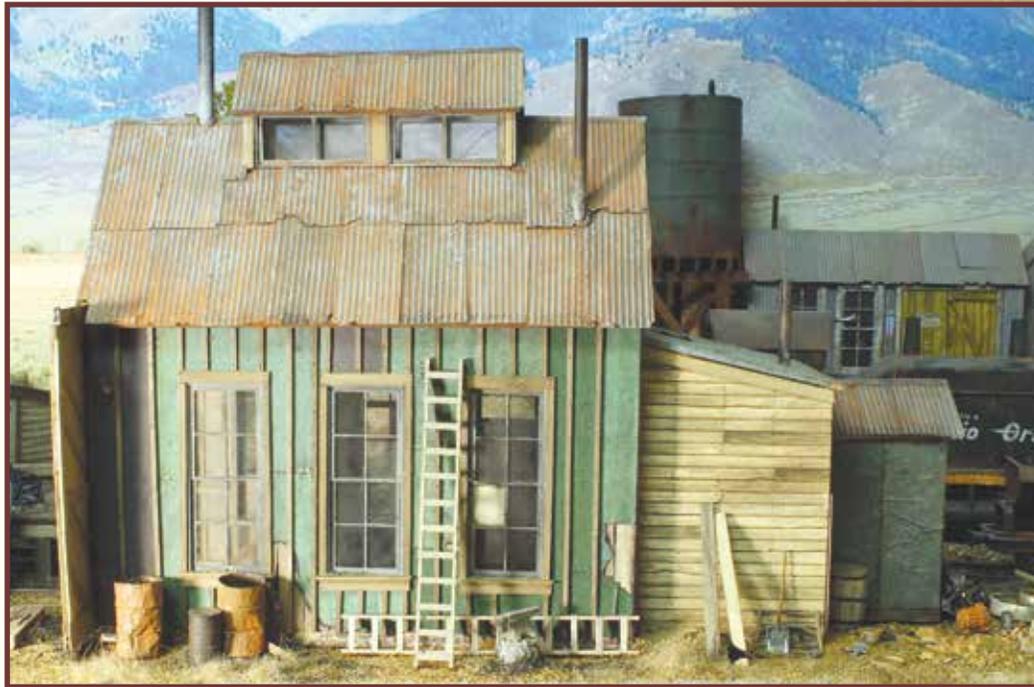
Below: The small village on the layout consists of scratchbuilt and kit bashed structures, all in 1/2-inch scale.



(text continued from page 26)

SPECIAL ANIMATED FEATURE

I wanted to be able to load tipper cars in the quarry. I suppose it's desire to return to the sandbox at my advanced age. I envisioned a fully operational truck-mounted dragline. However, my skills and mechanical knowledge were not up to the task. I am extremely fortunate to have a fellow modeler in the area, named Scott Betts, who happens to be both an excellent modeler and a competent mechanical engineer. He graciously agreed to give the project some life and designed an intriguing under-the-table mechanism that enables the dragline model to do about everything a prototype does. But that wasn't quite enough for Scott — he also installed sound! It still amazes me to be able to operate the controls of this contraption and load tipper cars. The basic dragline model is a die cast product from SpecCast, the Bantam C-35 Dragline, with the base chassis and tracks removed, mounted on a Tamiya Opel plastic truck chassis. My grandson, Grant Dunbar, 3D printed



Above: A side view of the compact engine shed, showing its multiple colors and additions.

Below: The animated dragline scoops up sand in preparation for depositing it in the tram car at right. The dragline works and has sound.



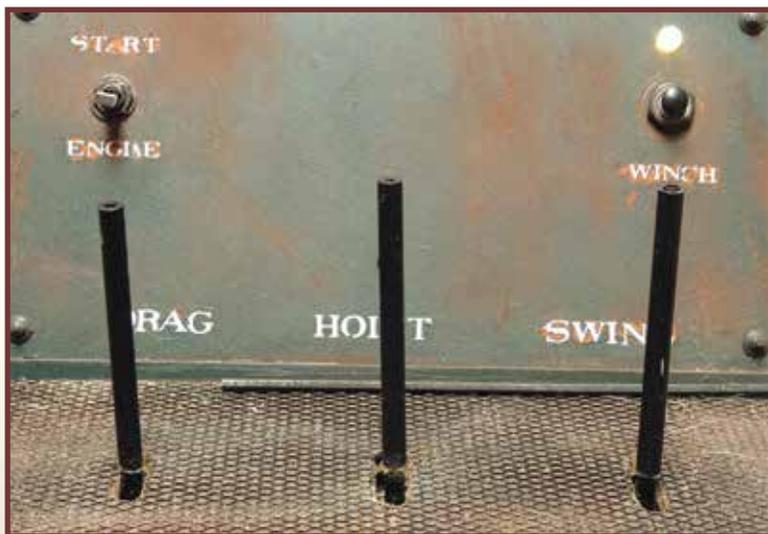
the screw jacks and shovels. The portable compressor and Caterpillar bulldozer in the quarry are models I built many years ago and re-used here. The compressor is totally scratchbuilt from styrene, basswood and brass, and the Caterpillar bulldozer is a die cast Conrad product that I detailed with an overhead blade mechanism.

SCENERY

The major scenic item on this layout is the photo backdrop. It was necessary to establish the locale. After a couple of failed attempts to piece together something I printed myself, I went to a Staples store with my photos, and they were able to stitch them together and

print them out on a continuous 22-foot-long roll of canvas-like paper. I painted the wall surface behind the layout sky blue and cut off the sky portion of the photo mural. With the mural installed on the wall, the layout is significantly expanded visually, and I am totally pleased with the final product.

I attempted to use static grass for the first time, with mixed results. In the end, my ground cover consists of some static grass interspersed with ground foam and hand-planted weeds and shrubs. An enormous double-trunked Ponderosa pine, fabricated by Will Cheshire, majestically occupies one corner of the backdrop. Other trees are made from various live and artificial plant materials, all dirt and rocks on the layout are the real thing, gathered near my Colorado home.



Above: The control panel for the dragline, constructed and suitably weathered by Scott Betts, who also designed and constructed the machine's operating mechanism.

Below: The quarry foreman stands in front of the Caterpillar tractor with overhead cable blade lift. This die cast Caterpillar 60 model, by Conrad, has been modified with a blade assembly and other details.



OPERATION

Operation is simple and consists of sending a locomotive down into the quarry to pick up a short string of loaded tipper cars, while dropping a few empties. Two sidings in the quarry assist in keeping empties and loads separate. The loaded cars are pulled out of the quarry and can be dropped at the passing siding near the quarry office for later delivery to the standard gauge connection. Tipper cars can be pushed up the embankment to the rock crusher and dumped there or can be pushed further along the embankment to be tipped into the standard gauge gondola. When the gondola is full, the body is lifted by hand off the fixed stationary trucks and dumped back into the quarry, and the cycle repeats.

CONTROL

My trains are run on simple DC power using a tethered throttle from GML Enterprises. Its central location on the front of the layout allows me to reach all the ground throws as I operate the trains.

CONCLUSION

This has been a very satisfying project, and I thank those who assisted in its construction. Having friends to share the hobby is an enriching experience. In addition to those named above, Gary Young and Steve Pollak were very helpful in completing this layout. 🚂

HOBO HAVEN



A Shelter In Place Project

by Chuck Lind, MMR
Photos by the author

The shelter-in-place was good for something. Lots of work got done in my train room, and I am getting caught up on many overdue items, such as my Hobo Haven. My model was built and taken to the Narrow Gauge Convention in Minneapolis, Minnesota in 2018. There, I promised Bob Brown that I would write something up and here it finally is!

I am a fan of run down, barely standing, structures, although I can only have a few unless I decide to model a ghost town. I had a place along my main line that just needed something to get a visitor to stop and take in the details. My decision was to build an old, wrecked box-car that had the trucks and other metal salvaged and then left to decay along the track. I searched *The Ameri-*

can Railroad Freight Car book by John H. White, for a plan of an early wood boxcar and chose an 1889 34-foot Burlington boxcar. The good thing about the drawings in this book is that they show all the interior bracing and building details needed to build the car. First thing I had to decide was how the car was wrecked. I wanted a boxcar damaged enough that they wouldn't have rebuilt it, so I decided that it would have one end badly damaged by breaking some of the main beams in the underframe, but mostly destroying the boxcar end above the frame. After making copies of the drawings in O scale, I started by distressing and staining the various sizes of wood I would be needing. The easy part was assembling the undamaged end of the car. However,



Title photo: The author installed his Hobo Haven in a spot on his layout just outside Tuolumne, close to the tracks where the hobos could catch a train.



Above: With the underframe completed, the author moved to building the boxcar sides. As he neared the wrecked end of the boxcar, he had to keep test fitting the sides so they would correctly match the damage of the underframe.

Below: The completed sides. The missing boards were temporarily attached to the side frame so they would have painting and lettering that matched. They were then removed and scattered in and around the wreck site.

it was not just a matter of building the car and taking a hammer to it. All the broken parts had to be planned based on support beams and framing that was still in place and not destroyed in the wreck (at this stage the hammer idea was looking good).

I started by building the car underframe. Since it was an early car, it was built with a wood center sill. Two of the side beams were broken and flooring was cut, weathered, and installed carefully breaking boards that would have

been affected by the wreck. With the underframe complete, the sides were laid out and built, making sure the parts followed the wreckage to the floor. I cut and stained the wood to be used for the side sheathing with Hunterline Driftwood. As I glued the boards to the sides, I randomly left boards off and broke the boards where needed. I went back over the model applying Golden Oak stain to the boards where they broke.

Before I colored the outside of the car, I temporarily replaced the missing boards





Above: End view of the undamaged end of the boxcar with some of the broken siding used to construct a canvas shelter over the hobo bath area.

so they would be consistently colored and lettered. I used Pan Pastels to color the sides using Red Iron Oxide for the top portions and Ochre for the heavily weathered lower portion. I found some dry transfers from Clover House for the Calvert, Waco & Brazos Valley that once ran where I live in Texas. The dry transfers were applied then colored with Pan Pastels to take the new look off the lettering, and were brushed with a wire brush to distress the lettering even more. At this point I pulled off the boards that I had temporarily placed in the sides saving them to be used elsewhere.

As I was building, I came up with the idea of a hobo camp. I figured that the hobos would make use of any kind of structure they could find along the tracks while they waited to catch that next train. So now that the car had a purpose, things changed a little as I built the car. Some boxes were left inside the car, and a hammock was strung to try and give the wrecked car a new life as a haven for the hobos traveling through the country.

Below: Completed sides and end, painted, lettered, and weathered, finally attached to the frame.



Right: The C. B. Hutchins and Sons company designed a roof that was popular in the 1890s. It used 2 layers of wood with the joint lines offset with a layer of painted canvas between the layers of wood, to make a waterproof roof.

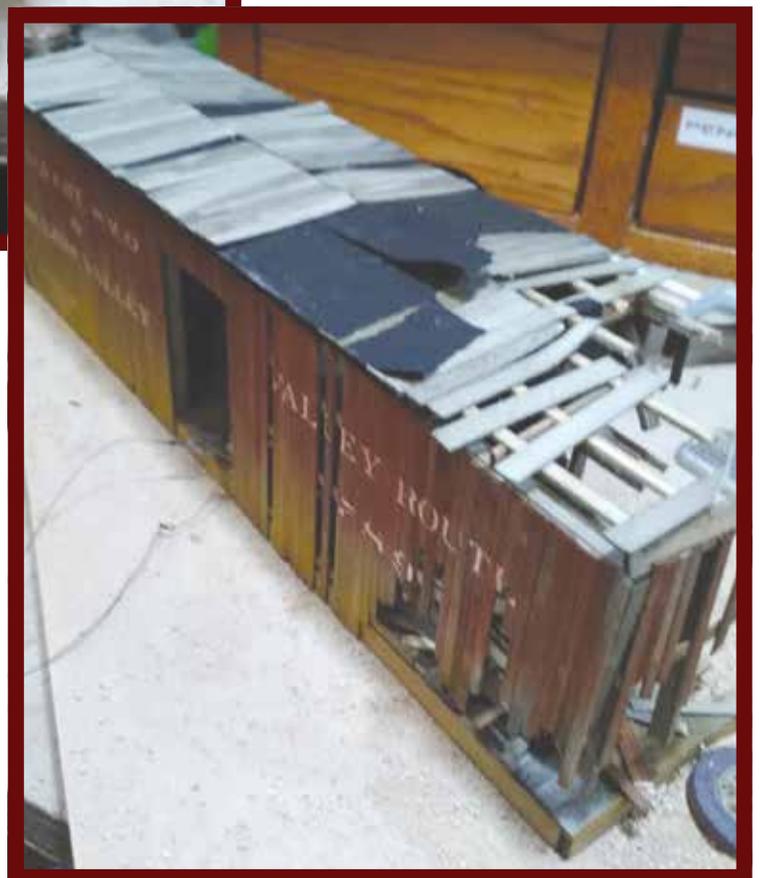


Above: The roof carlines were notched for the stringers that would run the length of the car. They were then carefully broken, matching the sides and frame of the wrecked end of the boxcar.

Right: Finished C. B. Hutchins & Sons style roof. One layer of wood attached to the stringers then a layer of painted canvas was attached over the wood. Over this a final layer of wood was attached, being careful to offset the joints of the wood from the first layer, giving the car a waterproof roof.

The roof was next. I had decided to build a C. B. Hutchins style wood double-sheathed roof which is basically one layer of wood covered with painted canvas, and another layer of wood over it with seams offset so it didn't leak. First step was to lay out the carlines and cut the notches for the stringers that ran the length of the car. The first layer of wood was applied. My canvas was made by cutting some 24# bond into strips and then spraying it flat black with a spray can. The second layer was applied over this. As I neared the end the wreck, boards were broken and adjusted to fit.

At this point, I mounted the boxcar to a piece of Gatorboard so I could add additional details. A base coat of scenery was added, planting the car into the landscape. From here, the fun began. I added a tarp over a rock bath area the hobos

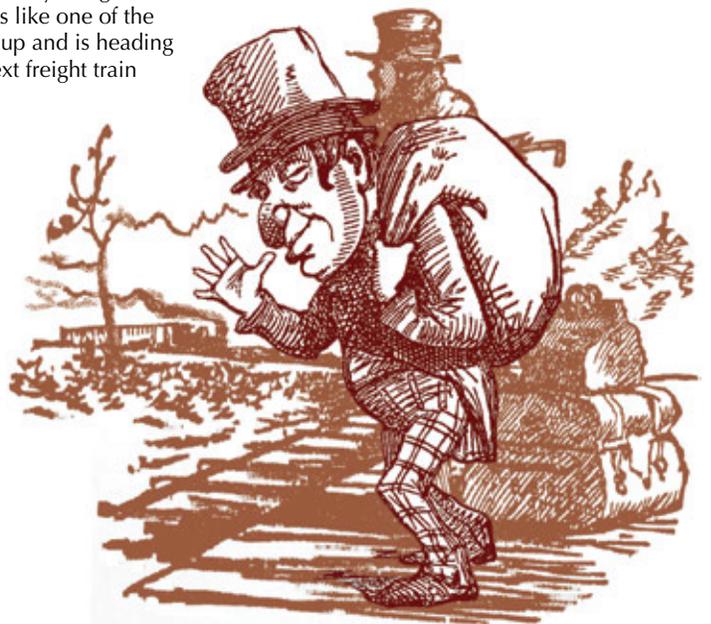




had constructed. For this I used some of the siding pieces to construct the framework for the tarp. Rocks were laid out for the campfire area with a flickering LED used to make the fire a little more realistic. Most of the left-over siding and all the wood I broke, was placed in the wreck area along with some damaged items the boxcar was carrying. A tree was planted, and a few evenings were spent adding weeds around and up through the broken framing. Hobos were added to complete the scene, along with a couple of birds on the roof.

I was very pleased with the comments I received on the model, and it took First Place in its category at the 2018 Narrow Gauge Convention in Minneapolis, Minnesota. 🚂

Above: Adding the details is the author's favorite part of scratchbuilding; it really brings things to life. Looks like one of the hobos has packed up and is heading out to catch the next freight train leaving Tuolumne.



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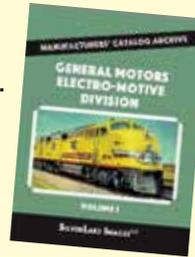
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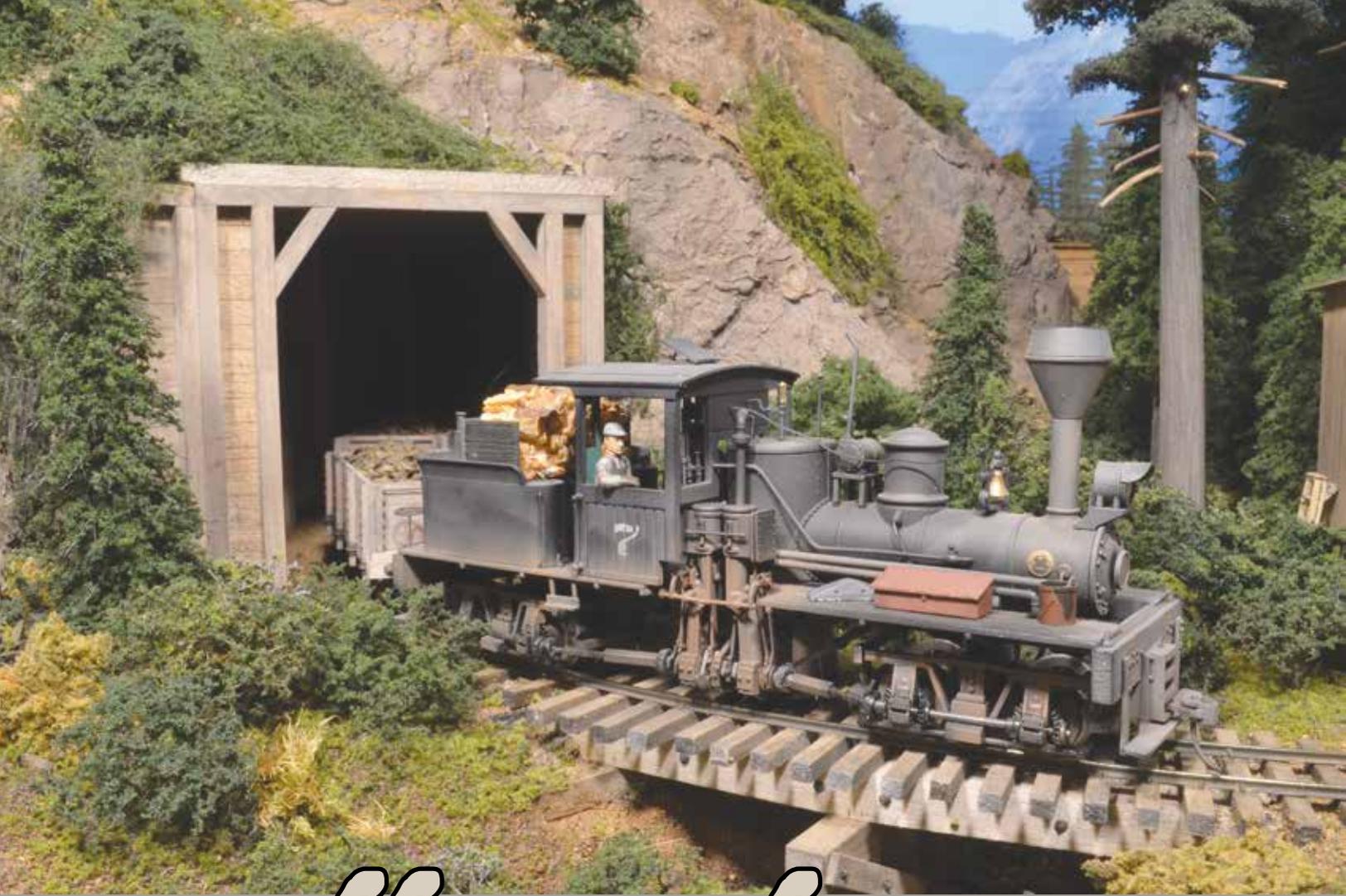
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Wallace Flats

A Portable On30 Display Layout

by Phil Holden
Photos by the author

A while back I needed a diversion from my HO scale logging layout (see September/October 2017 *GAZETTE*), so I decided to try the world of On30. Since there was a need for a small On30 display layout to take to our local train shows, and I wanted to show that you could build a highly detailed layout in a small space, I decided to go ahead with my small layout and name it Wallace Flats after my late father who was a proud supporter of my model railroad endeavors.

Construction Of The Base

Construction began with a 26- x 44-inch base framed with 1- x 4-inch boards covered with a piece of Masonite and a layer of 2-inch-thick pink Styrofoam. Some contours were cut into the Styrofoam followed by an oval of hand laid code 83 rail on wood ties.

I decided to add a mountain and tunnel on one end and started with the detailed tunnel portal and interior walls. The detailing of the interior walls was done

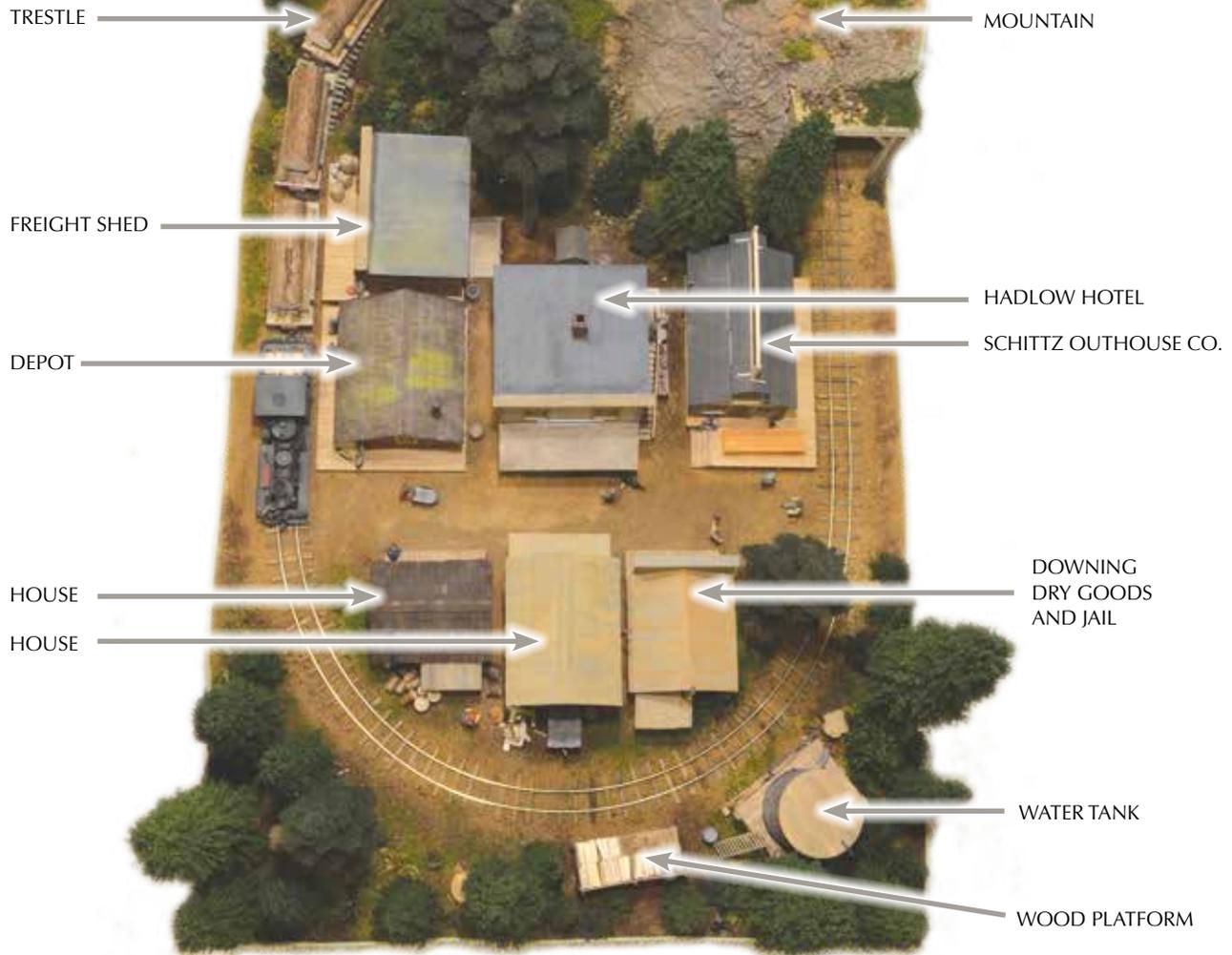
to add an interesting touch to the scene. I then used a lattice work of cardboard strips to form the mountain's contours. I covered these strips with plaster cloth and used a variety of rock molds to apply dental plaster castings. Staining the plaster rock formations was done with diluted acrylic paints in spray bottles, in a variety of pigments to achieve the finished look.

Basic scenery layers of real dirt were applied to the contoured base and were glued down with a mixture of white glue and water with a drop of detergent. After

Title photo: The author started detailing his layout with this tunnel portal.

Wallace Flats

AN On30
PORTABLE LAYOUT
26 x 44 INCHES



Above: There is a lot of modeling in this 26- x 44-inch layout.

this, I began developing a theme and planning the structures for the layout.

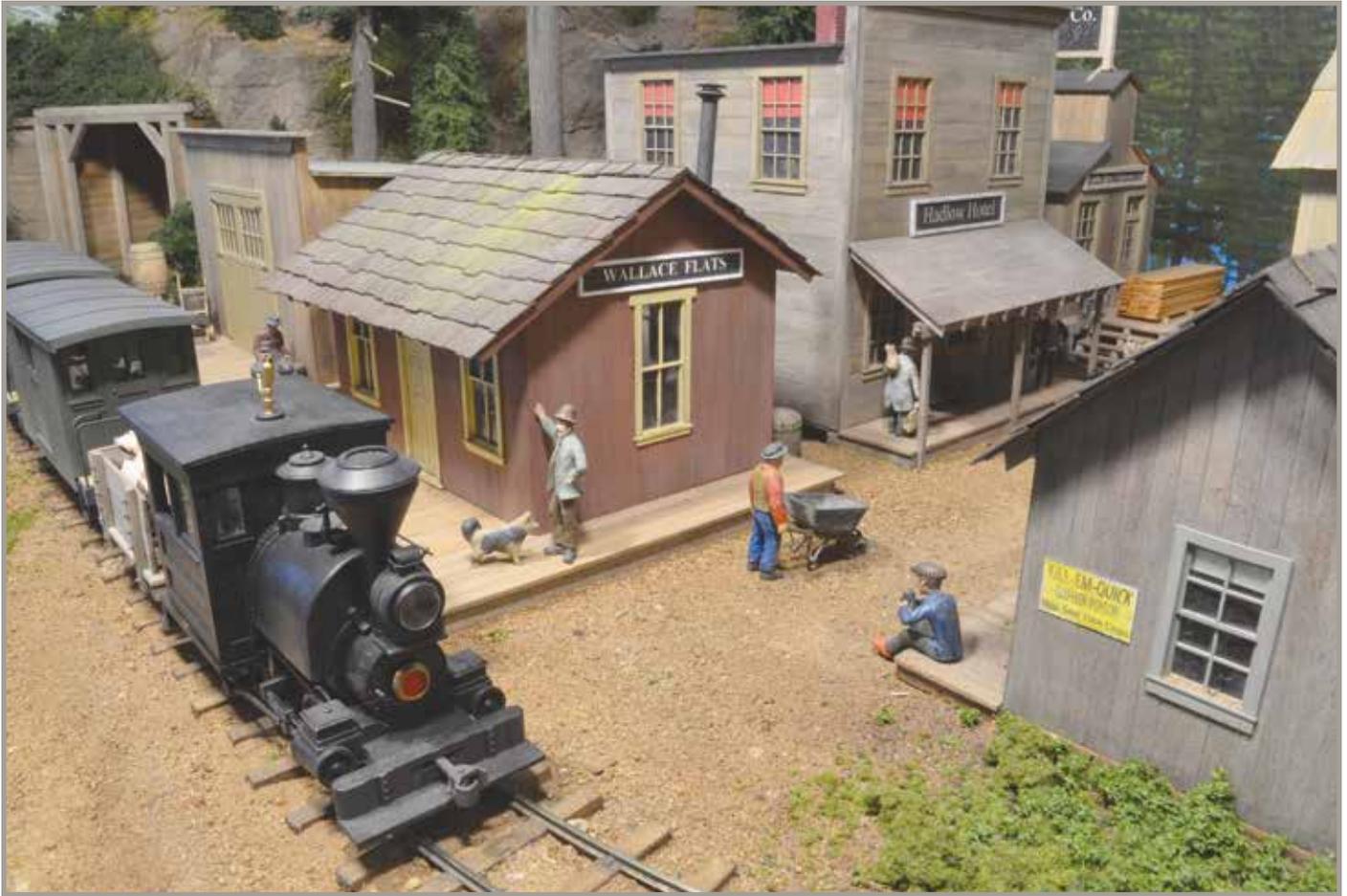
The Structures

I wanted to portray a small western town in the early 1900s so the structures needed to look right. Some of the buildings feature the traditional false fronts adding a western flair to the scene. There is a freight house next to a small

depot becoming one of the many focal points on the layout. In behind these two structures there is a hotel/watering hole with lots of detailing around it. Across the street there are a couple of houses and the Downing Dry Goods store/town jail. Apparently, the residents are a rowdy bunch, at least they don't have far to go from the Hotel! Next to the Hotel on the back side of the track's loop there is a manufacturing company that builds outhouses. I wanted some humor on my

layout and the name of this business is a small play on words. Since I am modeling the steam era, there is also a small water tank and a platform for wood.

Construction of the structures began with a base of 0.060-inch-thick styrene on which an outer skin of individual wood boards were glued. I use an inner styrene base on all the structures to add stability and make them rigid. Grandt Line windows and doors were used along with whatever other details I could



Above: The Wallace Flats depot.

Opposite top:
A back yard scene with detail.

Opposite bottom:
Looking up Main Street. Note
the use of figures to add interest
in such a small space.

find. Roofing materials varied from tar paper, cedar shake and corrugated metal materials for an interesting look. Weathering of the buildings was done with an India ink and alcohol mixture along with some pastel/chalk pigments, and the odd bit of dry brushing.

Around each structure details abound, and a few interesting scenes have been placed to catch the viewer's eye. Figures are well situated to create a believable town atmosphere including the animal population. I have several ideas for more details that will eventually find their way to the layout. One detail I have been considering is a horse and wagon for the Outhouse company. A cargo of a small display of outhouses would be appropriate, maybe some HO scale outhouses might do the trick.

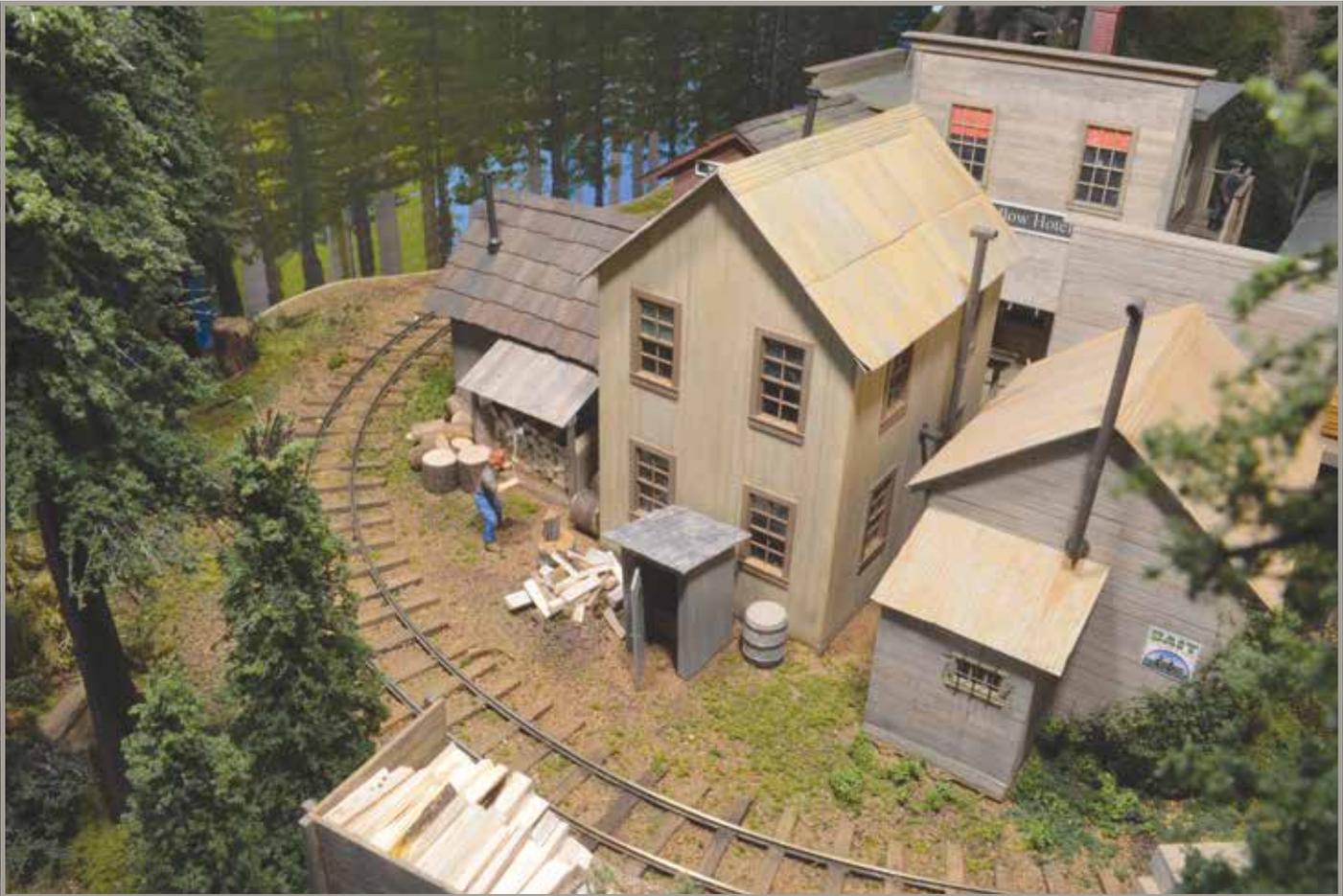
Scenery

Once the structures were placed, the fun began with the multiple layers of scenery material. I made up my own blend of forest understory by grinding up a blend of natural materials including leaves, bark, cedar bows and cedar pods

(text continued on page 40)

Below: The main industry.





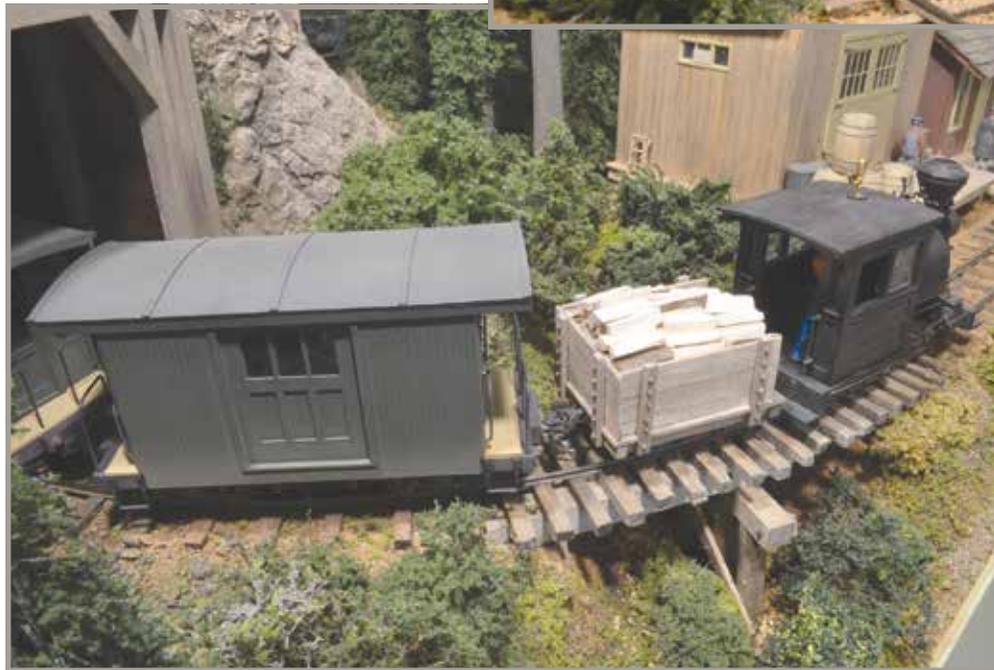
(text continued from page 38)

in a blender. For a finer grind I used an old coffee grinder. This understory was again glued down with the water/white glue mixture. I also used a variety of Woodland Scenics products including their coarse foam, clump foliage and fine turf in a mixture of greens. The tree trunks are all carved from cedar and detailed with Woodland Scenics foliage material. Some of the trees feature bottle brush foliage.

Several small bushes were made using thistle or hemp rope simply by splaying the strands outward, coating them with hairspray, and flocking with very short pieces of the same rope to build up the structure of the bush. After that was dry, I painted them a dark brown and applied layers of coarse turf. I also added extra texture by applying small amounts of



Above: The Shay squeezes around a curve into town.



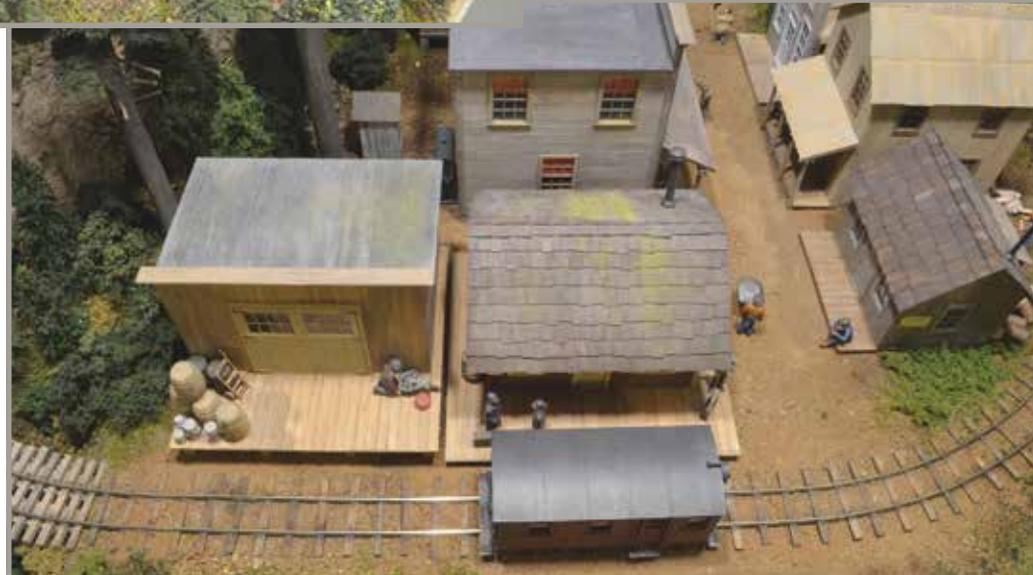
Left: Sharp curves need short passenger cars.

Below: The railcar stopped at the depot.

fine turf in a contrasting color for more definition; I even added a few ferns around the layout by using natural step moss (*hylocomium splendens*). This is a great natural material, and you can use the moss for your fir tree branches to create very realistic foreground trees.

Motive Power And Rolling Stock

The layout is powered by a simple DC power pack that plugs in via a guitar input jack at one end of the layout. The main motive power is either a Bachmann

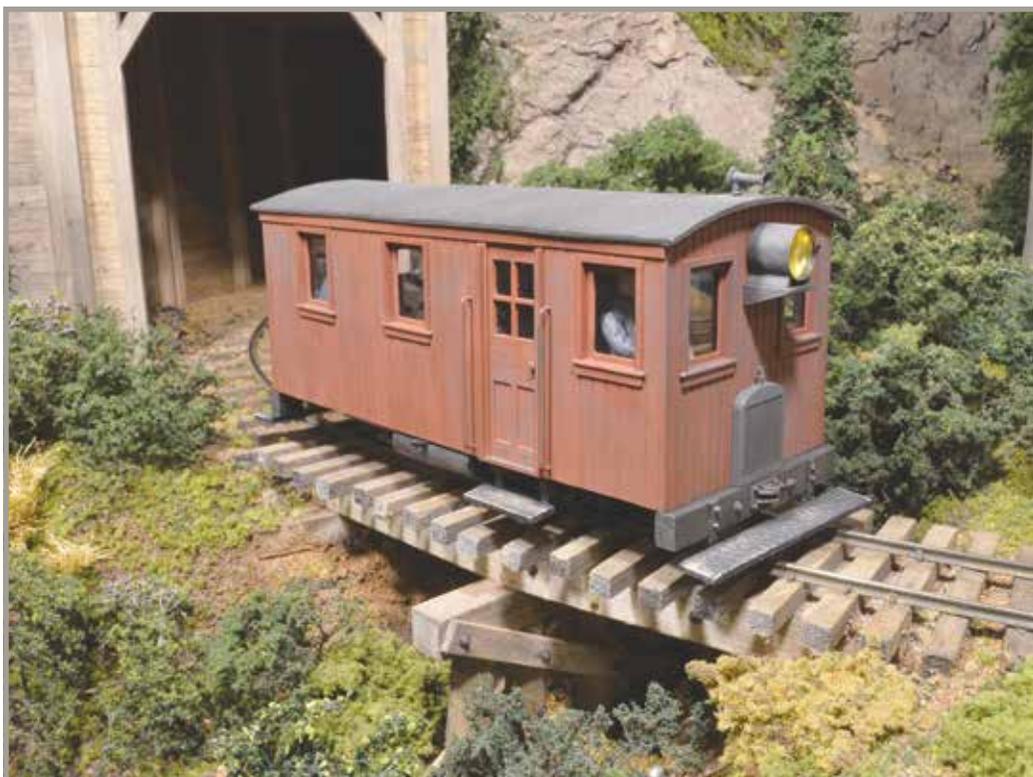


2-truck Shay or a pair of Bachmann Porters. I also have a logging speeder that was kit bashed from a 4-wheel Athearn EMD Model 40 Diesel (see sidebar on page 42). This was an interesting project to build and has proven to be a great runner.

The freight cars are a mixture of log flats, ore cars, short flatcars, and a couple of cabooses. I even have a small two-car passenger car consist I can let loose. All the freight and passenger cars are scratchbuilt as I prefer to create my own rolling stock.

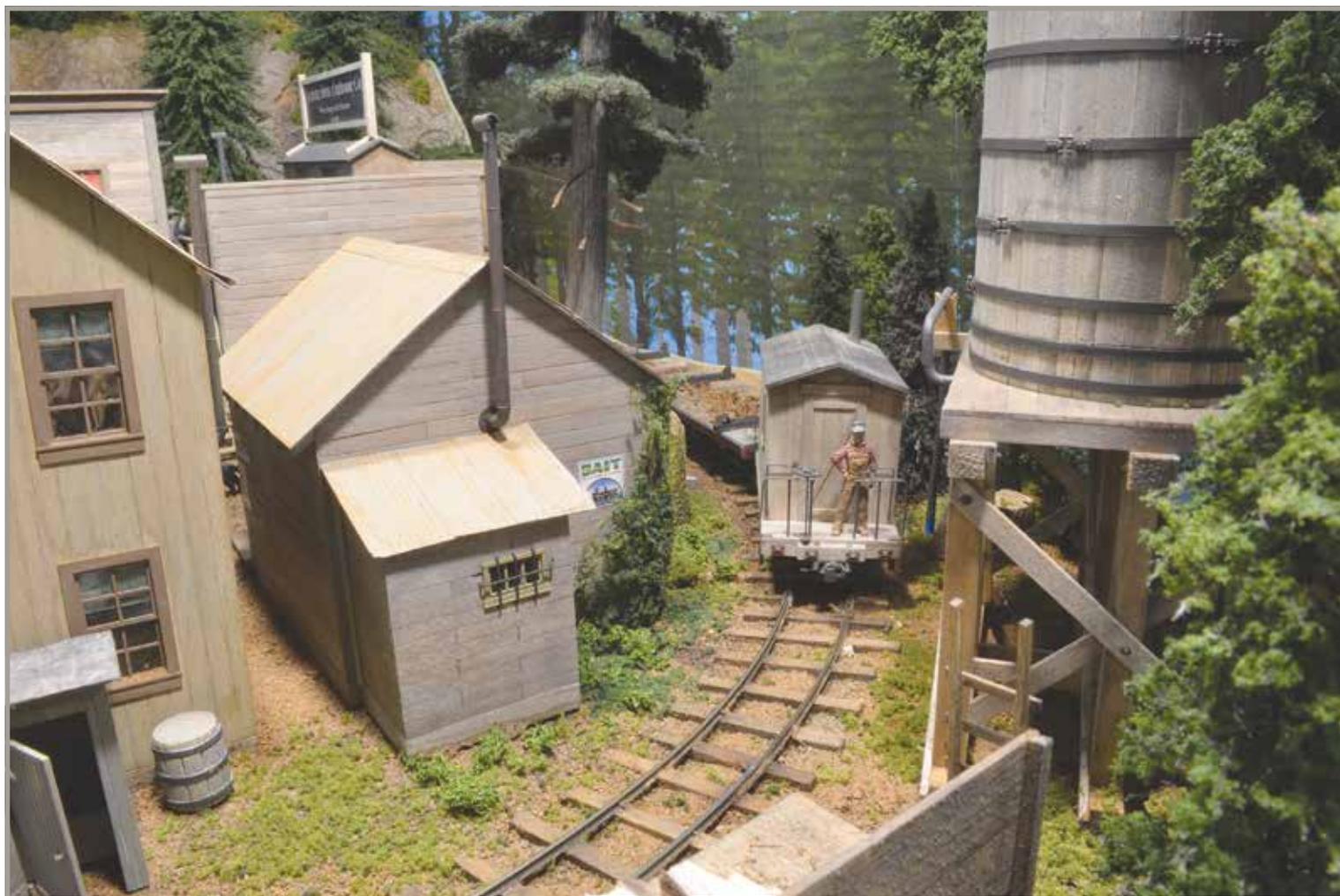
Conclusion

The display layout is still a work in progress, as is any layout no matter the size. After all that is where all the fun is, adding and/or subtracting to create new scenes and details. Give it a try and let your creativity flow, you will enjoy every minute. 🚂



Above: The railcar on its way to the Wallace Flats depot.

Below: One of the logging cabooses passes the water tank.



On30 Crew Speeder For Wallace Flats



by Phil Holden
Photos by the author

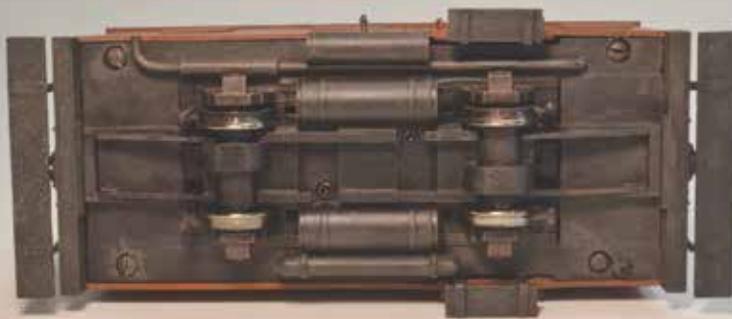
I have always wanted to build a logging speeder and built a static model of one several years ago. In my search for a mechanism, I found a HO scale four wheel Athearn EMD Model 40 Diesel locomotive with the dimensions I was looking for.

Once I removed the Diesel's shell, I could see I needed a new floor with a narrower opening for the Diesel's mechanism. I made a new floor from sheet and strip styrene to fit the mechanism.

Next, I built the speeder's shell from V Groove Styrene siding. It is 17 scale feet long, 7 feet wide and 6³/₄ feet high. I added Grandt Line caboose windows and doors for the crew. I even added the heads of sewing pins for the doorknobs. The roof is sheet styrene covered with masking tape to represent tar paper. Other details include a fabricated headlight bracket and headlight with an MV Amber lens from my junk box, an HO scale single chime horn, and a Grandt Line Reo Radiator, along with the appropriate handrails to finish the body.

I glued two benches for the crew and a seat for the driver in the interior, and covered the Athearn mechanism with electrical tape painted grimy black. Then I added wood end beams and footboards with Rio Grande Models #3535 sled plates to create the look of link & pin couplers. I added smaller footboards with brass flat bar stock for the supports under the side doors.

I completed my speeder with a crew and some weathering. All in all, it was a relatively simple project at a reasonable cost, you should give one a try! 🚂





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LOCOMOTIVES OF THE WHITE PASS & YUKON ROUTE

Part 1: Alaska's First Locomotives

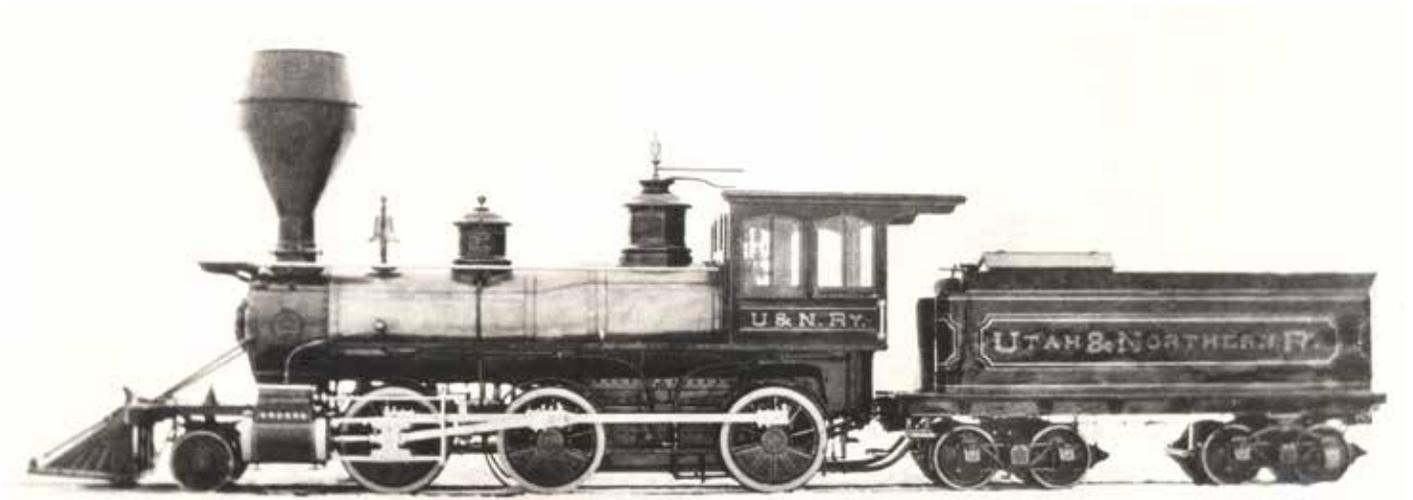
by Rob Bell

Title photo: WP&YR #52 is seen crossing bridge 2a with a baggage car and a coach. Number 2 was rebuilt in 1900 with a new steel boiler and larger cylinders and renumbered 52. Number 1 was also rebuilt in 1900 and became #51. Number 52 lasted on the White Pass until 1931 when she replaced #51 when it broke down on the Taku Tram. Number 51 was repaired and lasted on the White Pass until she was retired in 1941. *Photo, Skagway Museum Talbot collection, high-resolution scan by Chuck Morse, courtesy of Bruce Pryor.*

The story of the White Pass & Yukon Route (WP&YR) is one of steep grades, incredible scenery, bitter cold, and a stubborn gold miner's attitude. The railway started with the gold lust of the Klondike Gold Rush of the mid-1890s and a need to get people and supplies to the far north. The 110 miles from the Alaskan boom town of Skagway, north to the town of Whitehorse in the Yukon Territory (YT), was completed in two-years, two-months, and two days. This feat was accomplished with British financial backing, Canadian engineering,

and mostly American labor. The locomotives of the White Pass & Yukon Route were an interesting assortment from the beginning. In this series I will explore the early locomotives of this unique and fabled railroad, accompanied by beautiful drawings from long-time GAZETTE contributor, David Fletcher. I start with the first locomotives in the Alaska Territory, WP&YR numbers 1 and 2.

The first locomotives to arrive in the Alaska Territory were built in 1881 by the Brooks Locomotive Works of Dunkirk, New York for the Utah & Northern



Above: Utah & Northern #23 as-built by the Brooks Locomotive Works in 1881, inclusive of the original Pennsylvania Railroad inspired livery, dating to 1868, which had become popular with a number of locomotive firms by 1880. Number 23 would go on to become the WP&YR #1 in 1898, and last on that line until retirement in 1941. Photo, collection of David Fletcher.

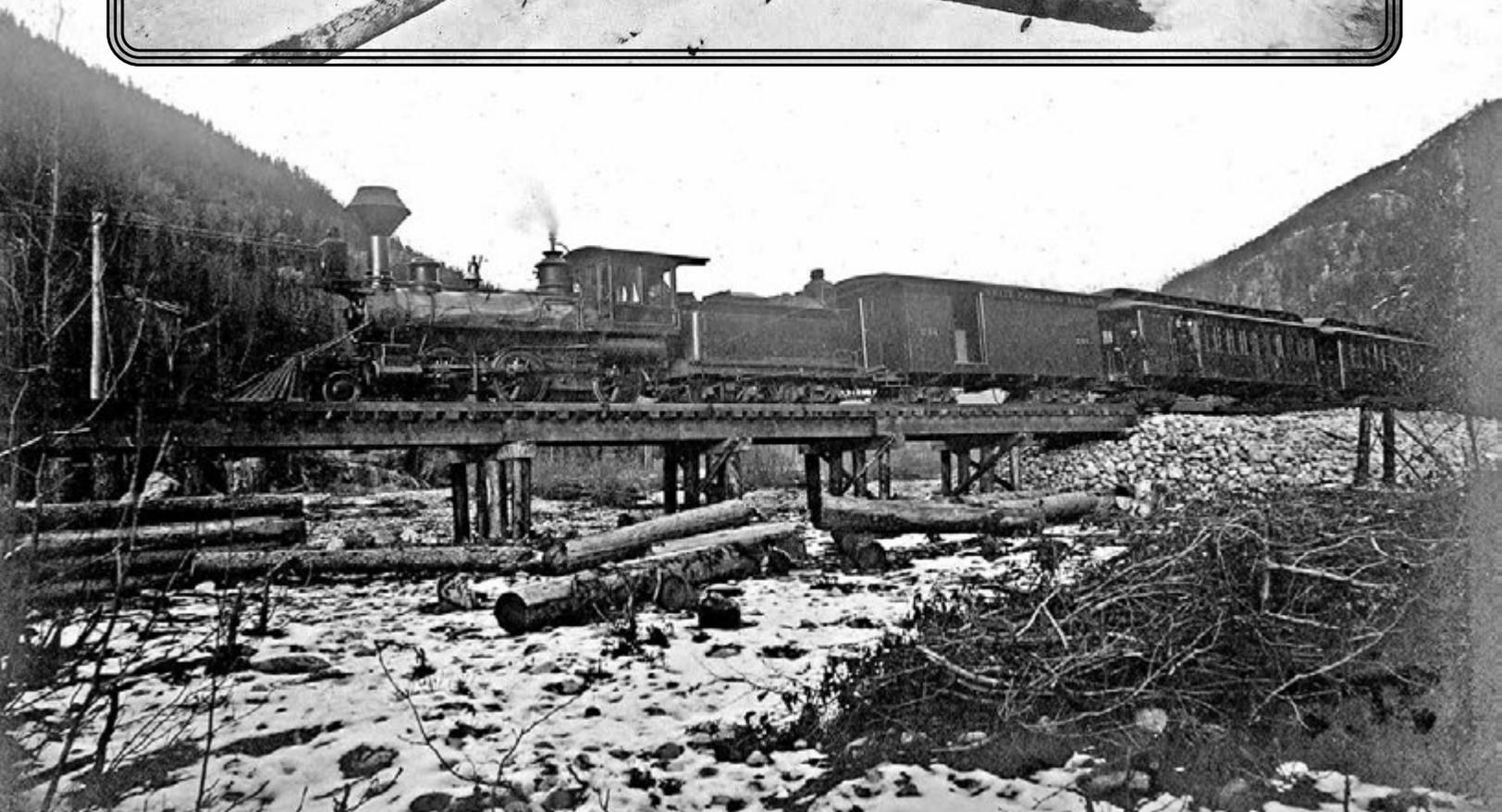
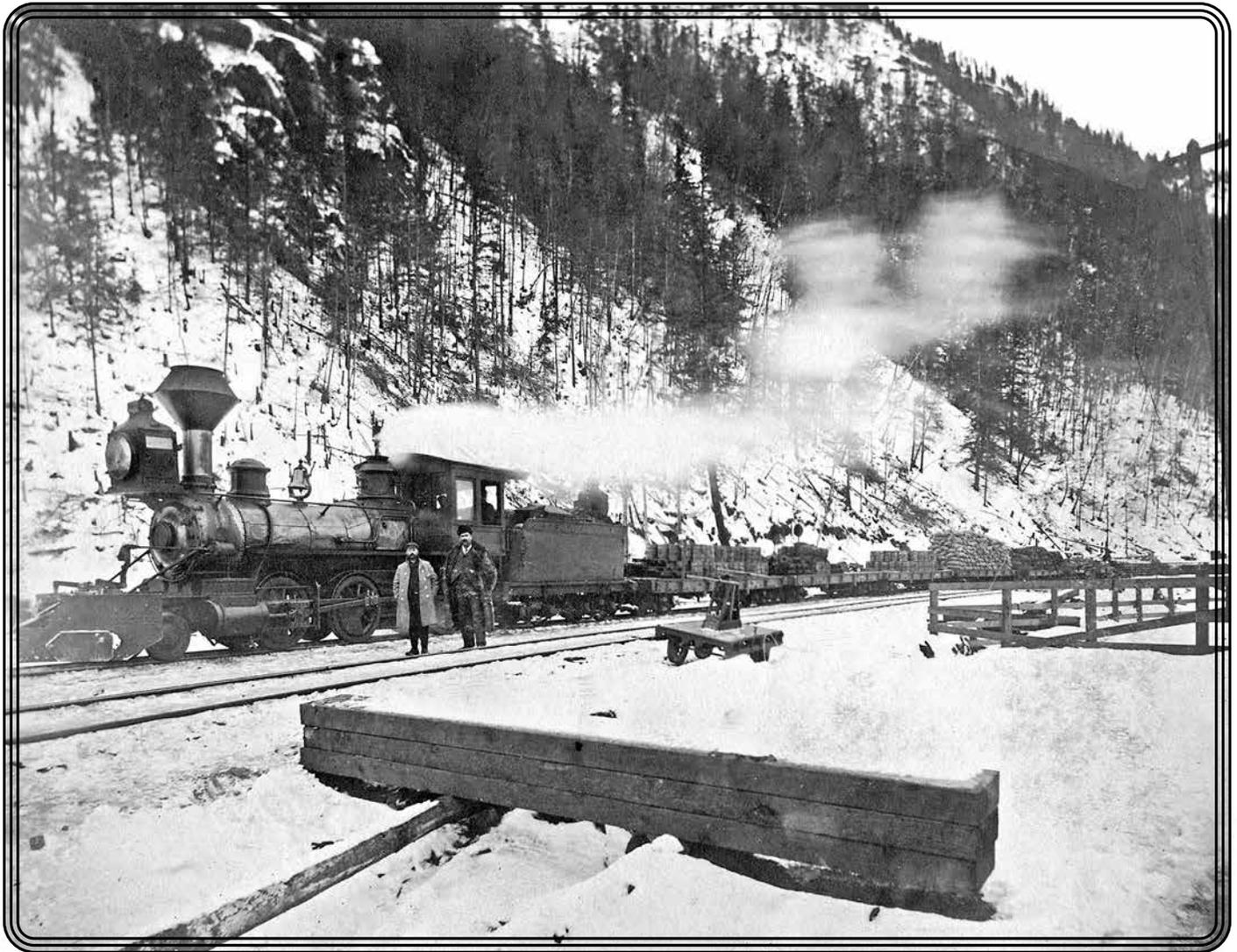
Below: WP&YR #2 prepares to haul the very first train, full of enthusiastic townsfolk, 4 miles north out of Skagway on July 21, 1898. Number 2 was the first locomotive in the Alaska Territory when she arrived in Skagway on July 20, 1898. This first excursion train also made #2 the first locomotive to operate so far north. Photo, H.C. Barley, Skagway Museum Dedman Collection, high-resolution scan by Chuck Morse, courtesy of Bruce Pryor.



Railway (U&N) as their numbers 23 and 37. Both were 2-6-0s with 42-inch diameter drivers, and 14- x 18-inch cylinders. The U&N was a narrow gauge railway spur off of the Union Pacific section of the transcontinental railroad at Ogden, Utah. Originally, the Utah Northern Railway (1871–1878), foreclosure led to its sale to Jay Gould and the Union Pacific who changed the name to the Utah & Northern Railroad. Capital was poured into the line and by the end of 1881 the railroad had been extended all the way to Butte, Montana, nearly 600 miles from Ogden. It was

(text continued on page 47)





(text continued from page 45)

during this infusion of money that the 2-6-0s were ordered from Brooks. The locomotives were renumbered in 1885 to the UP's numbering system, #23 became #80 and #37 became #94. But their usefulness on the line was short-lived as the railroad was widened to standard gauge in July 1887. Both were sold in November 1889 to the Columbia & Puget Sound Railroad.

The Columbia & Puget Sound RR (C&PSRR) began as the 3-foot gauge Seattle & Walla Walla Railroad and Transportation Co. (S&WW) in 1873. The Oregon Improvement Company (OIC) bought the S&WW in 1880 and renamed it the C&PSRR. The OIC was a subsidiary of the Northern Pacific and improvements to the line were facilitated during the mid- and late-1880s. This is when the two ex-U&N locomotives were acquired by the Columbia & Puget Sound. U&N #23 became C&PS second #3, and #37 became C&PS second #4. The Columbia & Puget Sound was standard gauged in 1897, and again the locomotives were up for sale.

Due to the Klondike gold rush, which was already in full swing, construction of the White Pass & Yukon Route began on May 28, 1898. On July 20, 1898 the first locomotive in the Alaska territory arrived and was unloaded from a barge in Skagway, Alaska. The next day, the same locomotive; the White Pass & Yukon Route's #2, the ex-U&N #37, ex-C&PS #4, steamed north out of Skagway pulling a few flatcars full of exuberant townsfolk

over four miles up grade. Number 2 was also the first locomotive to operate so far north. I am unclear as to whether #1 arrived on the same barge, or days or even weeks later, as the White Pass purchased a total of five locomotives from the Columbia & Puget Sound in 1898. Construction continued in the harsh conditions through the winters of 1898 and 1899 with these locomotives hauling construction materials and excursion trains. Two new boilers for the two aging Brooks engines arrived onboard the AL-KI, on September 9, 1899.

Locomotives 1 and 2 were rebuilt in 1900 with these new boilers accompanied by new cylinders. New, 46-inch diameter steel boilers replaced the aging 44-inch diameter wrought iron, wagon-top boilers. The fireboxes were raised and widened, and the new cylinders (14½- x 18-inches) were installed to fit the larger and higher boilers. The boiler pressure was increased from 135psi to 185psi, (although, a folio diagram from 1908 gives a boiler pressure of only 160psi). Along with the rebuilding in 1900, the White Pass set about renumbering its fleet of locomotives, #1 became #51, and #2 became #52. The new boilers and cylinders must have been well worth the money and effort as both of the moguls out lasted their ex-C&PSRR sisters on the White Pass. Number 51 stayed on the White Pass until 1920, and #52 until 1931.

Another gold strike occurred in July 1898 near Atlin, British Columbia. To ease access to the gold fields, the 2½-mile-long Atlin Short Line & Navigation Company wooden tramway opened in June 1899 across the isthmus near Taku Arm to Scotia Bay on Atlin Lake. In June 1900, the White Pass & Yukon Route bought out the assets and completed the line, now known as the Taku Tram. With no way of turning the locomotives, they had to run forward from Taku to Scotia Bay and backwards for the return. This short line had a maximum grade of 7 percent and a single 12-ton, open passenger car. When the Tram's first locomotive was taken out of service in 1920, the White Pass brought in old locomotive #51. In 1931, #51 blew out a chunk of her main steam pipe, and they replaced her with #52. Photos show that only the locomotives were swapped, #52 using #51's tender while on the Taku Tram. The White Pass repaired #51 and used her until retirement in 1941. Number 52 was the motive power on the Taku Tram until 1936 when she was stored in Atlin, British Columbia;

replaced by a Westminster Iron Works gasoline powered locomotive.

Number 51 was outside the Whitehorse, YT engine house when the engine house burned down in December 1943, destroying numbers 10 and 14, two ex-East Tennessee & Western North Carolina ten-wheelers. She evidently escaped with little or no damage and was displayed in 1958 at the MacBride Museum in Whitehorse, Yukon Territory, and remains there today.

A volunteer group called Project 52 retrieved #52 from Atlin in 1964 using a Caterpillar tractor and sled to get the engine across the frozen surface of Atlin Lake, and then a low-boy truck to get her back to Whitehorse. After a thorough greasing of the bearings, the 80 year old engine was brought back to Skagway behind White Pass Diesel locomotive #92 and a volunteer crew. Number 52 was stored next to the Skagway roundhouse when the roundhouse burned on October 15, 1969. Fortunately, only the tender's deck and frame were damaged. She was put on display at the United Transportation Union Hall in Skagway in 1971. In 2000, #52 was taken to the WP&YR shops for asbestos removal, where the shop crews removed the lagging from her boiler. Number 52 was cosmetically restored in 2014 and placed on display near the Skagway depot, where she can be seen today. In 2019, Chuck Morse began restoration on the tender deck and frame.

It is truly amazing that these two locomotives each gave over 45 years of service, and they both survive to this day — over 140 years since they rolled off of the Brooks Locomotive Works assembly floor. Oh! But the stories these locomotives could tell if they could only talk, stories of the sourdoughs and the Gold Rush, stories of the construction of "The Scenic Railway of the World," stories of the battle to construct the 1500-mile Alcan Highway, stories of almost unimaginable cold, and so many more stories.

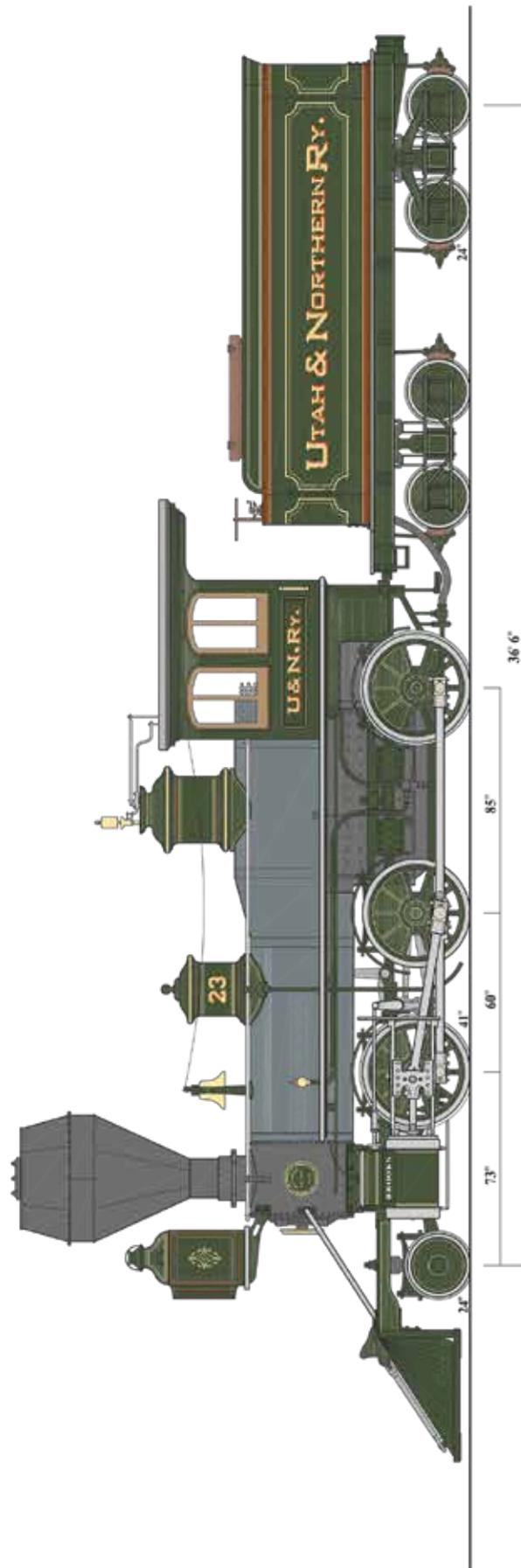
I would like to thank Bruce Pryor, Robert Hilton, John Stutz, and Boerries Burkhardt, for their assistance and generosity with photos and information. In the next issue, I will investigate an early D&RG connection with the White Pass accompanied by another fantastic drawing by David Fletcher. ■

Opposite top:

WP&YR #1 with a train load of construction materials ready to head north, circa 1898. Number 1 can be identified by the location of the injectors on the side of the boiler. Number 2's injectors were farther forward. Of course this changed when the locomotives were rebuilt with steel boilers in 1900. Note the flanger actuator mounted on the side of the smokebox. Photo, collection of Bruce Pryor, high-resolution scan by Chuck Morse.

Opposite bottom:

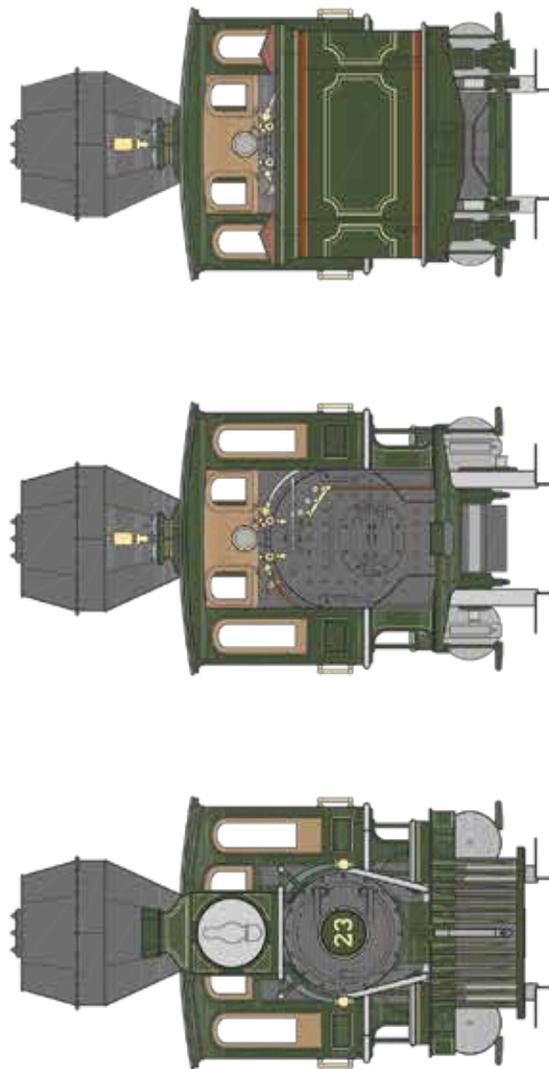
The first passenger cars arrived in Skagway on September 10, 1898. Here #1 pulls a passenger train across the Skagway River sometime after their arrival. Photo, H.C. Barley, collection of Bruce Pryor, high-resolution scan by Chuck Morse.



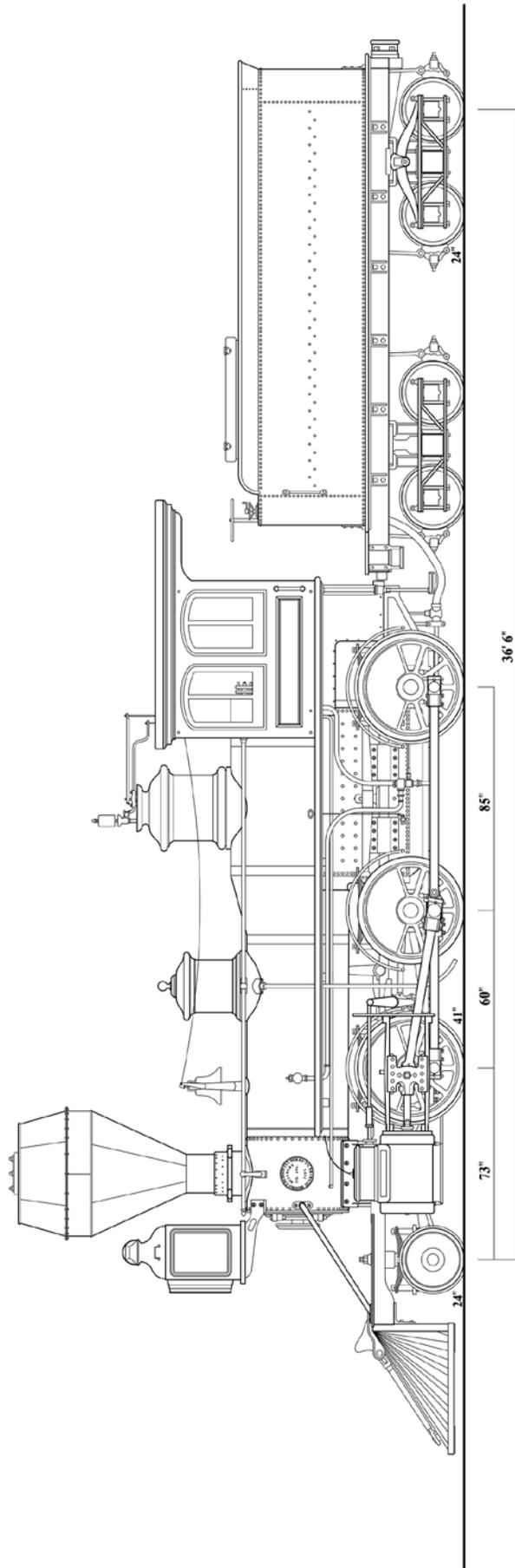
**UTAH & NORTHERN RAILWAY, 2-6-0 No. 23
BROOKS LOCOMOTIVE WORKS C/N 494, 1881.
3' GAUGE**

RECONSTRUCTED USING U.P. DATA, JUNE 1 1885
MODIFIED DRAWING - NARROW GAUGE LOCOMOTIVE
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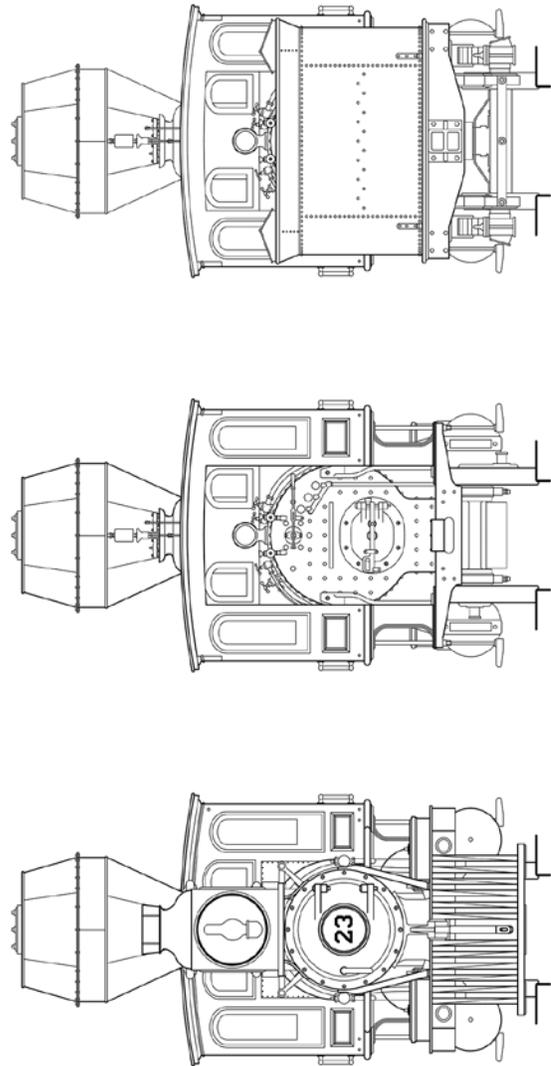
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COLORADO & SOUTHERN RY. NARROW GAUGE COACH #62

CHANGES IN LETTERING AND DECORATION 1899 TO 1935

C&S second class coach #62 was built by the Pullman Palace car Company in 1889 to plan 701 for service on the Denver & Rio Grande narrow gauge lines. These “no frill” tourist sleeping cars had basic plain wood slated seats and sleeping facilities.

After the conversion of the D&RG to standard gauge, Pullman sold these narrow gauge tourist sleepers to many different railroads. In 1896 the Union Pacific, Denver & Gulf Ry. purchased this car from Pullman and promptly rebuilt it into coach #193 with red plush seats and ungraded interior woodwork. The UPD&G also repainted the car an “Olive Green” color and decorated the exterior with gold painted lining and flourishes.

The newly formed Colorado & Southern Ry. inherited this refurbished coach in 1899 and renumbered the car #153. Based upon photographs of other C&S narrow gauge coaches of this era, I believe coach #153 kept it’s decorative gold paint and lettering.

In 1906 the Colorado & Southern renumbered passenger cars with this coach emerging from the paint shop as C&S #62 without the decorative lining and flourishes. At this time all C&S narrow gauge coaches had the car number applied to both ends of the car centered above the trucks. Lettering was gold.

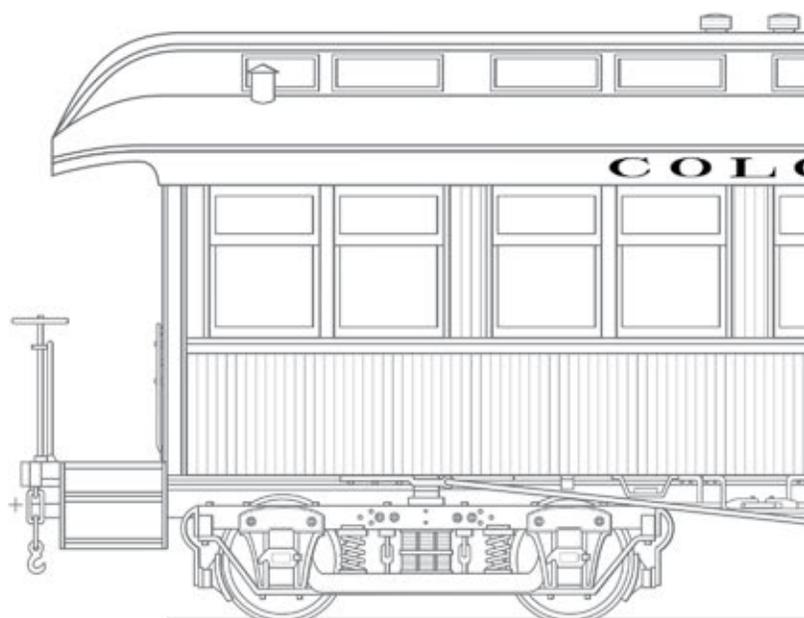
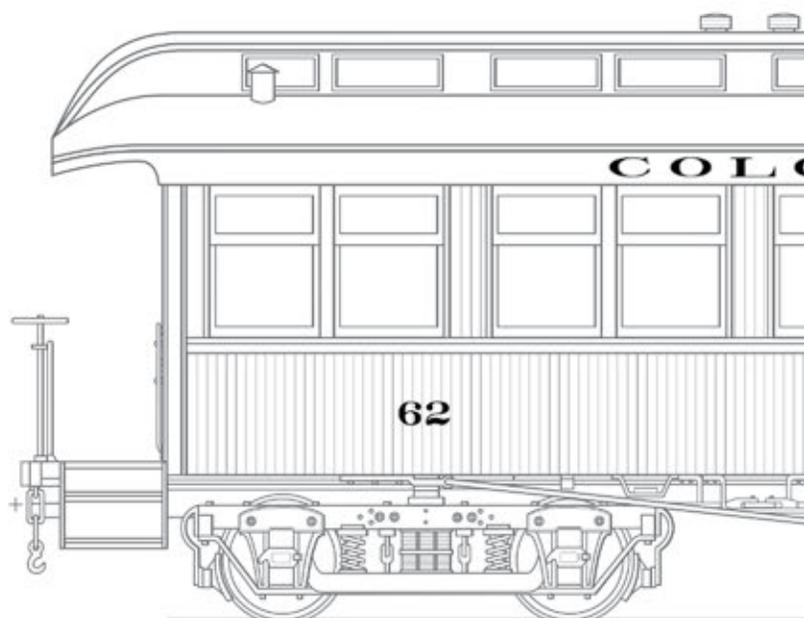
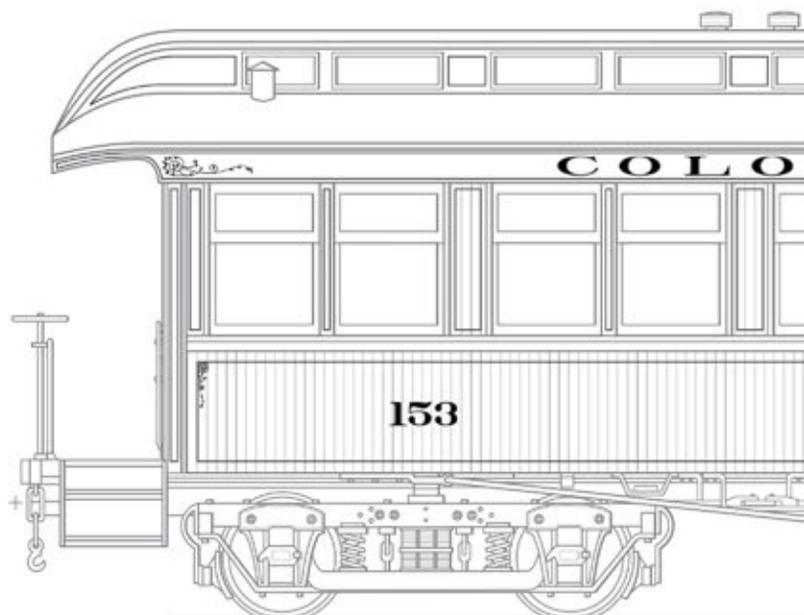
Sometime during or after the great war, the C&S narrow gauge passenger car lettering was simplified further with only one car number centered on the car side.

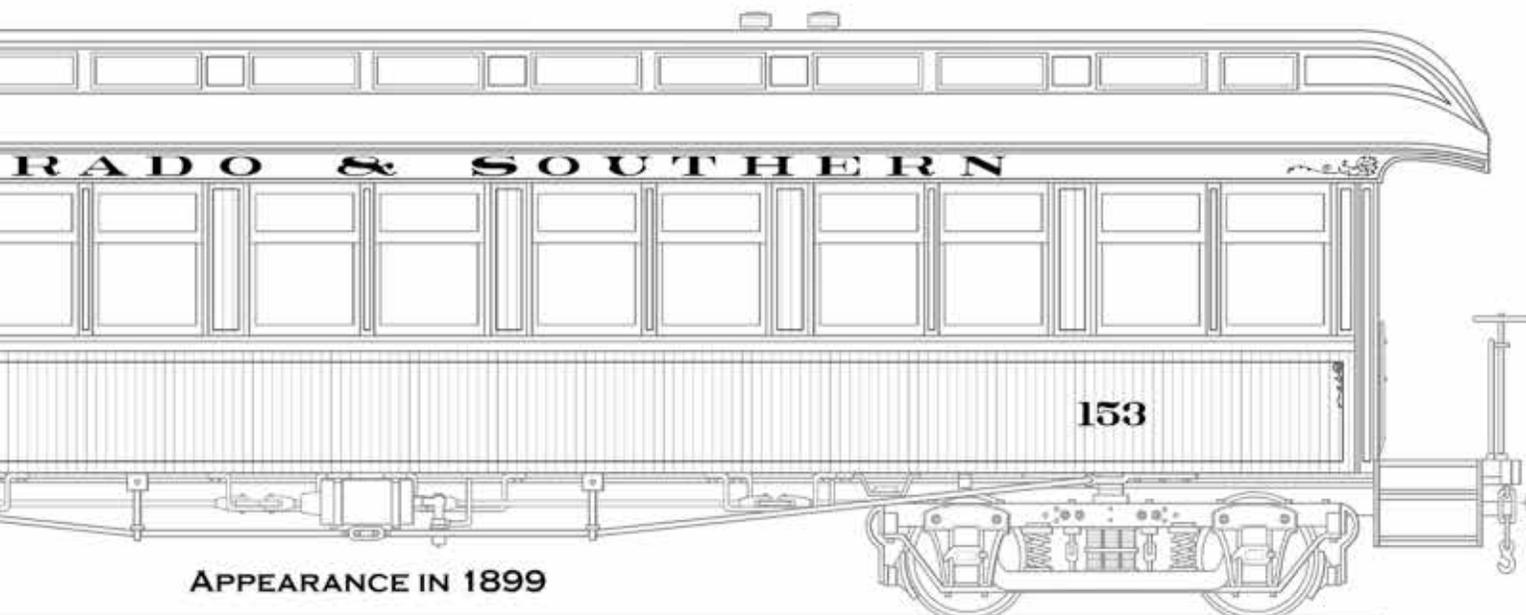
In 1938, C&S second class coach #62 was stripped down and converted to a bunk car for use during dismantling of the narrow gauge line. C&S coach #62 was dismantled in May, 1939.

DRAWINGS BY ROBERT STEARS © 2022

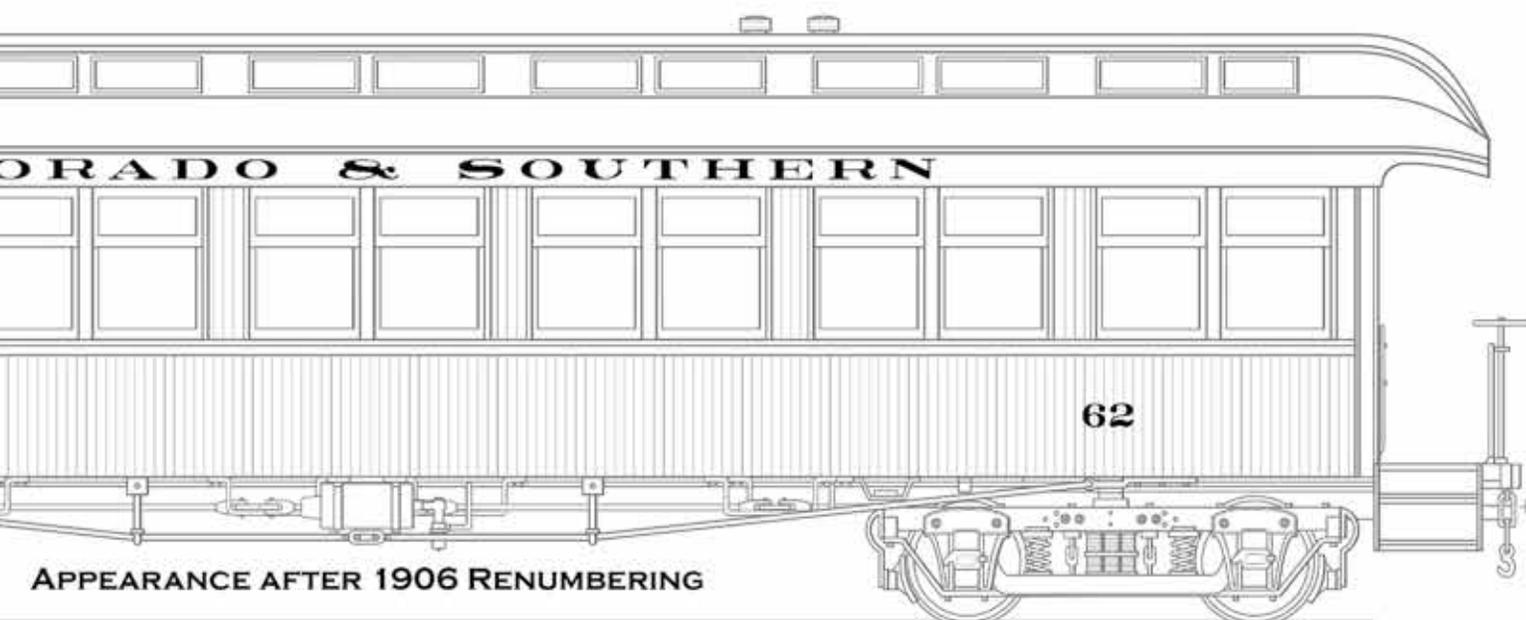


1/4 INCH = 1 FOOT

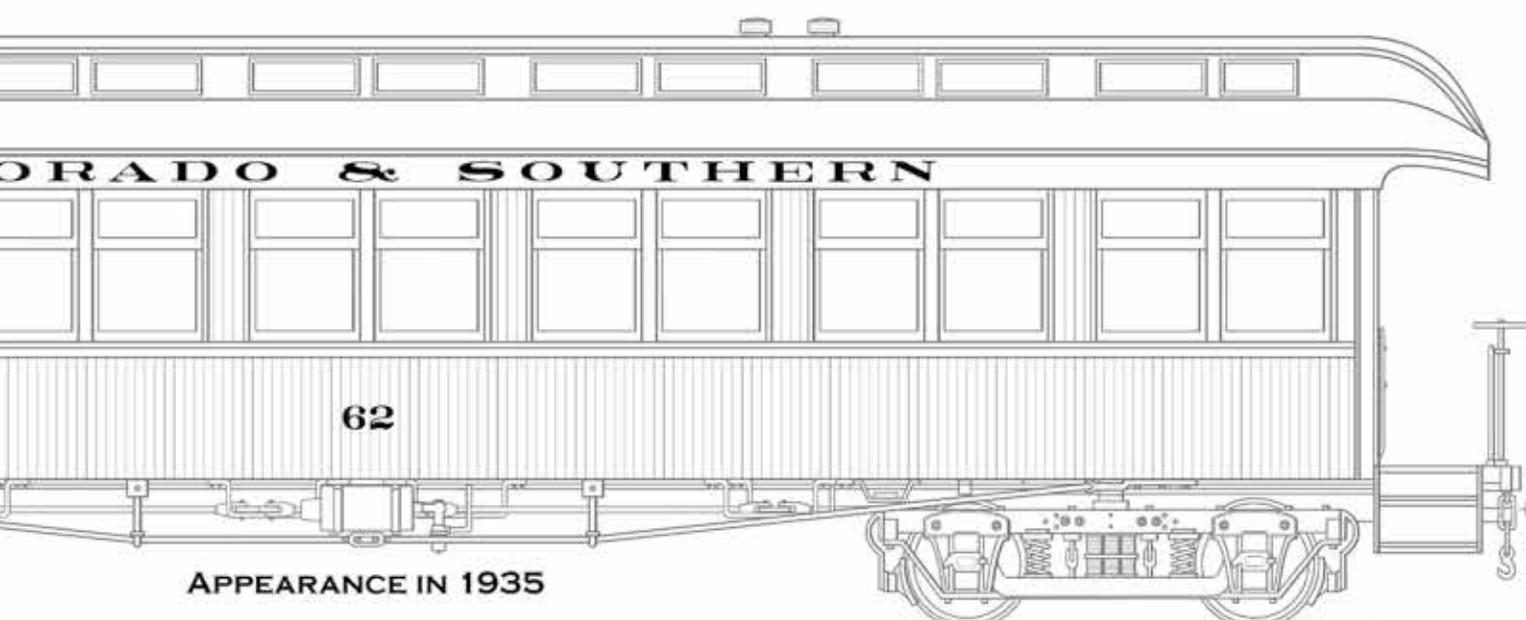




APPEARANCE IN 1899



APPEARANCE AFTER 1906 RENUMBERING



APPEARANCE IN 1935

OLYMPIA AND TENINO RAILROAD/ OLYMPIA AND CHEHALIS VALLEY RAILROAD

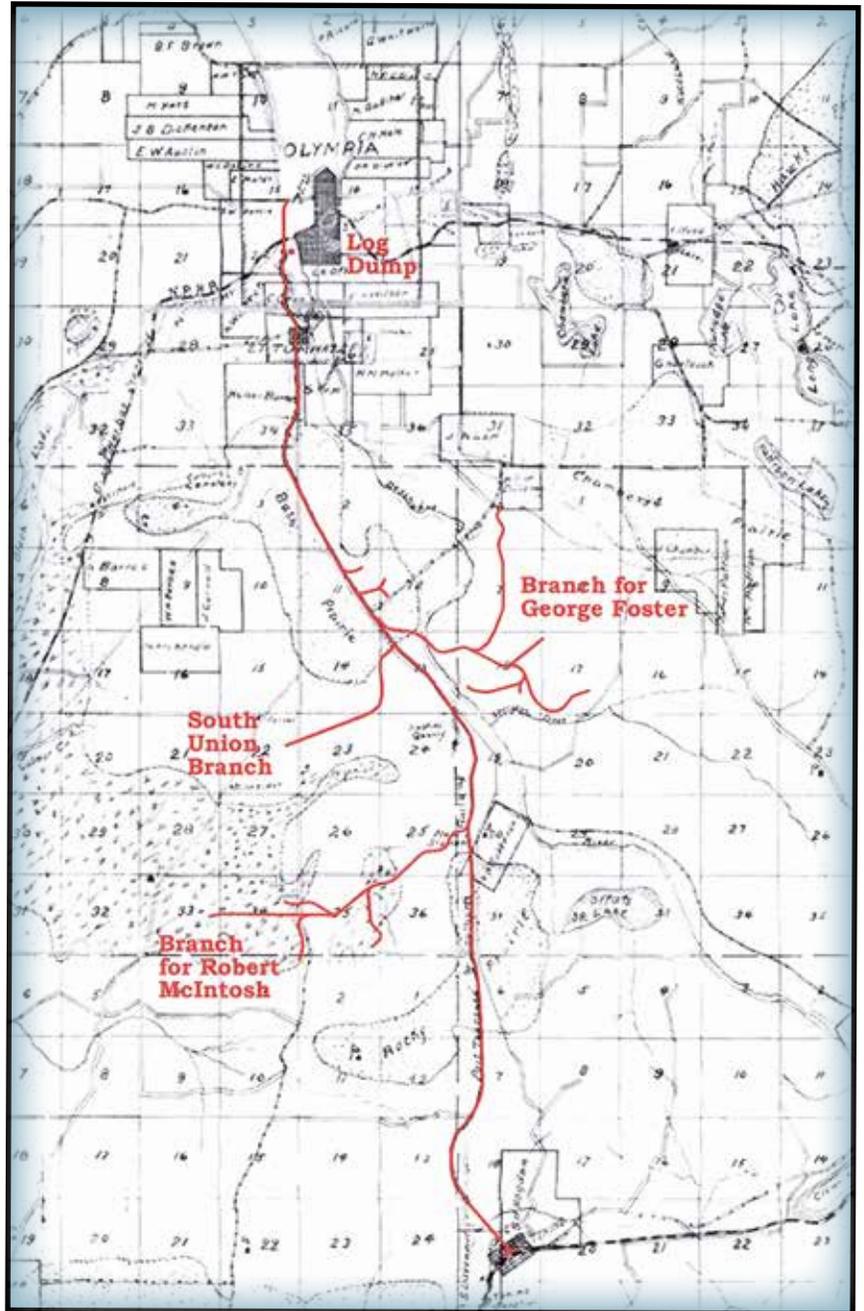
OLYMPIA, WASHINGTON'S FIRST CONNECTION TO THE OUTSIDE WORLD WAS A NARROW GAUGE RAILROAD

by Peter J. Replinger

In 1874, the closest mainline railroad to Olympia, Washington, was the Northern Pacific Railroad some 15.5 miles south at a village called Tenino. Because of cheaper construction costs, the incorporators of the railroad decided to build a 3-foot narrow gauge system. Originally constructed to haul coal, the railroad never was extended to the coal mines around Tenino and relied on just hauling logs to be dumped into Puget Sound for revenue. Two round trips a day for hauling passengers between Olympia and Tenino were also made.

The grading of the roadbed was begun on April 7, 1874, and it wasn't until July 1878 until crews began laying 30–35-pound rail on the grade. The railroad gained elevation from 36 feet at Olympia to 250 feet at Tenino. In May 1874, a new locomotive from Baldwin arrived by sea. She was a 4-4-0 named the E. H. OUIMETTE and was #1 on the system, having 42-inch drivers and a total weight of 43,300 pounds. On August 1, 1878, a passenger train from Olympia to Tenino made its first run. The official name of the railroad was the Olympia and Tenino Railroad Company.

Three different logging spur systems were eventually built off the mainline for three different operators. The first branch



OLYMPIA AND TENINO RAILROAD

NOT TO SCALE

MAP BY JAMES S. HANNUM, M.D.

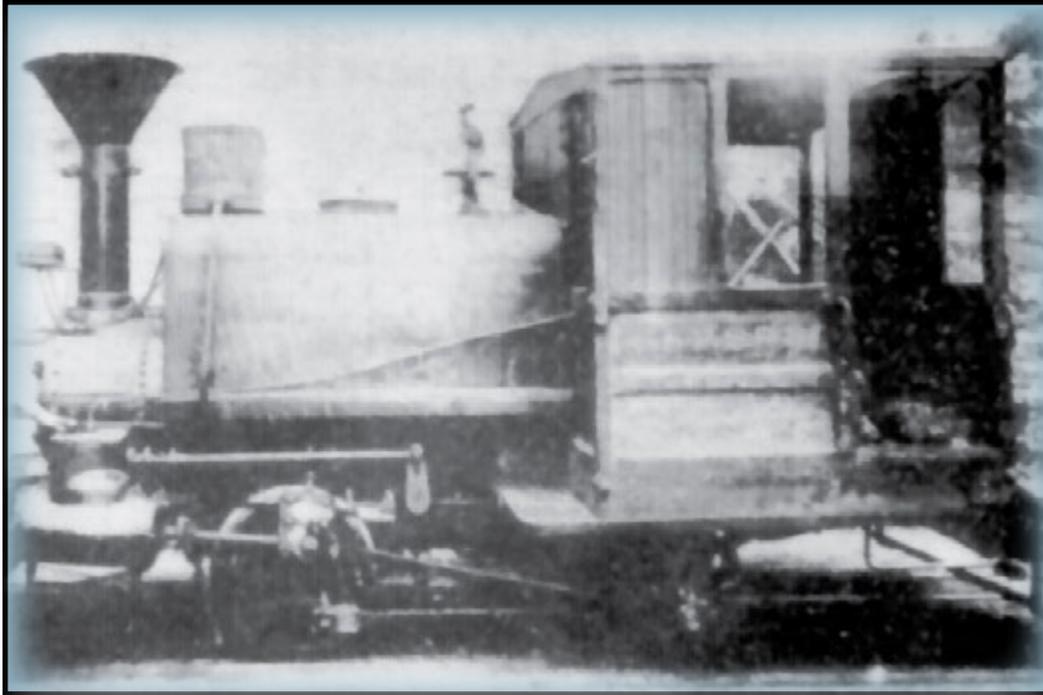
on the system was operated by logger George Foster. He employed a large force of men in cutting and barking the logs, and a number of teams in hauling the logs to the railroad. In 1882, Mr. Foster had a large body of timber in the region of his camp, sufficient enough for several years of logging. In that same year, the railroad was bringing in 55,000 board feet of logs

daily. Five cars were used, and three trips were made each day.

Next, the South Union branch of the railroad was operated by the Morris Brothers (George W., Philip Guy, Robert James, and Frank M.) who were affiliated with the Tacoma Mill Company. They employed the use of one of the Olympia and Tenino's locomotives (prob-

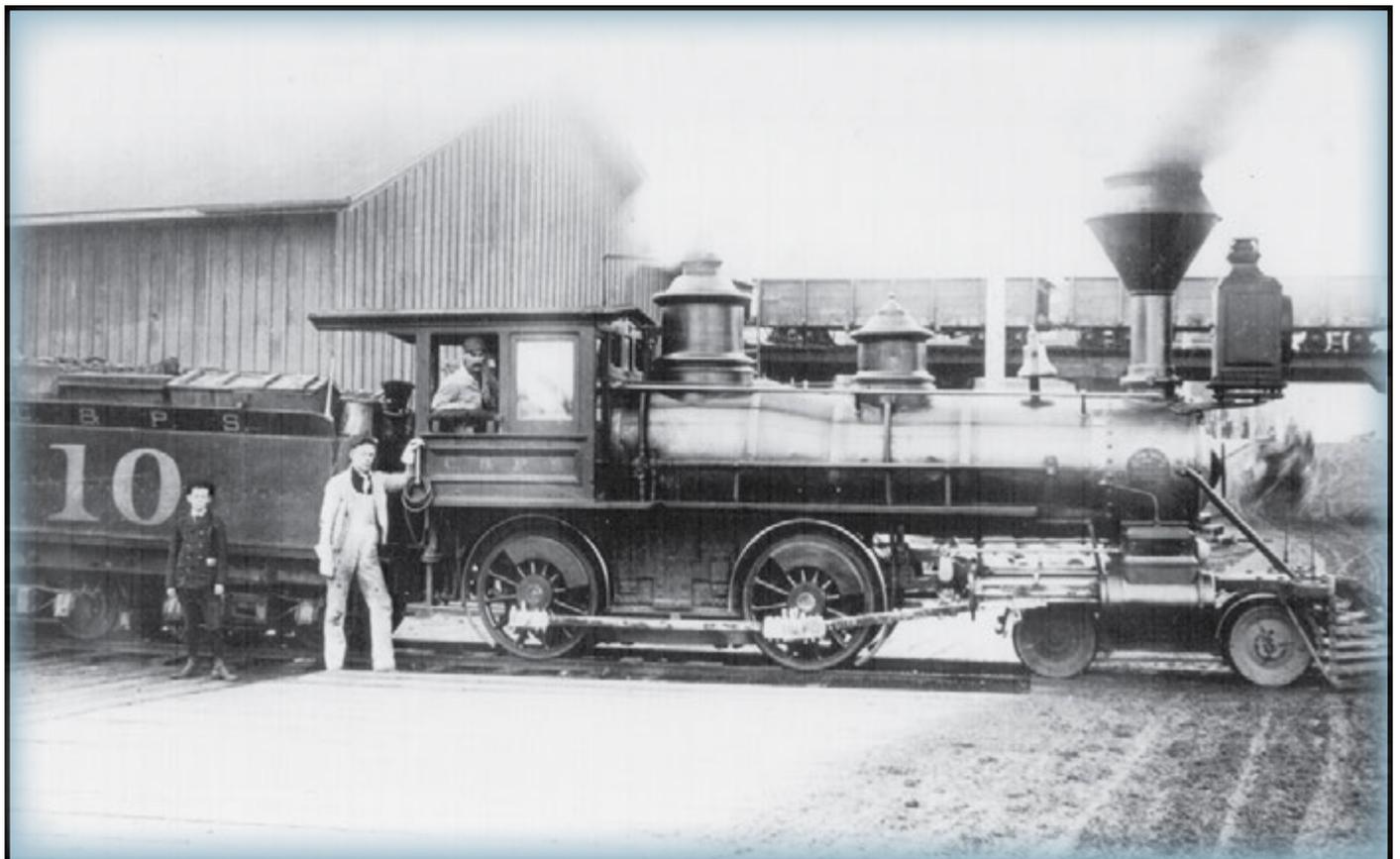
ably the first #2), which appears to be an 0-4-0 built by the Globe Iron Works of San Francisco with a flat front end suggesting she probably had a "gypsy" spool on the front of the frame. She was sold in 1887 to be used in the construction of the Puget Sound and Chehalis Railroad

(text continued on page 56)



Left: The WALLULA on the Columbia and Puget Sound Railroad.

Below: Photo taken after 1891 of the E.N. OUIMETTE in Seattle on the C&PS RR. Photo collection James S. Hannum, M.D.





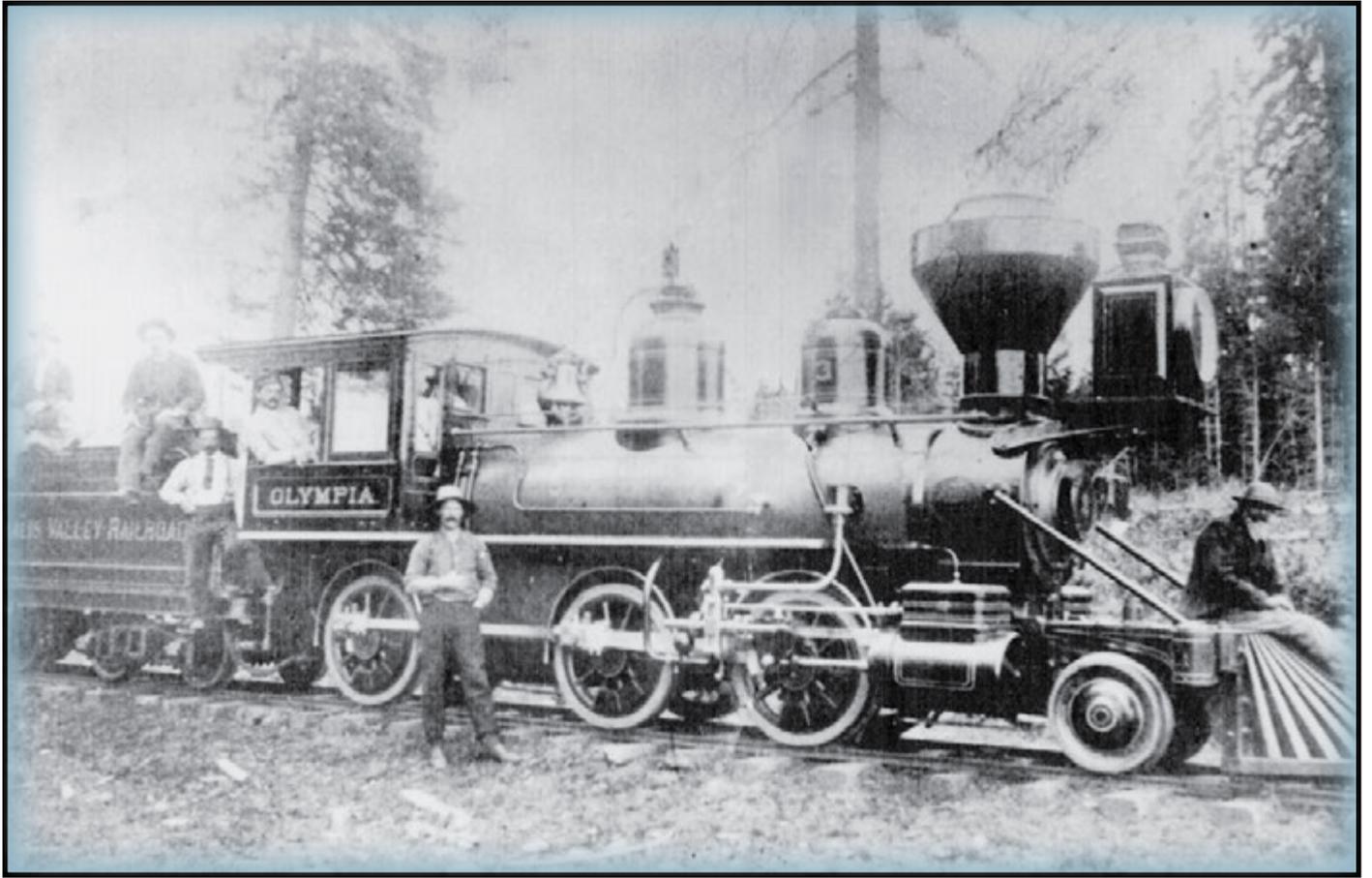
Above: Photo taken about 1891 of a 2-4-2T on the Port Townsend Southern Railroad depot in Olympia after the line had been standardized. The man standing on the ground is Mr. Morrow, the father of the late Charlie Morrow who bought and removed the SKOOKUM out of Deep River in the 1960s. The depot in the photo was just north of today's 4th Avenue bridge.

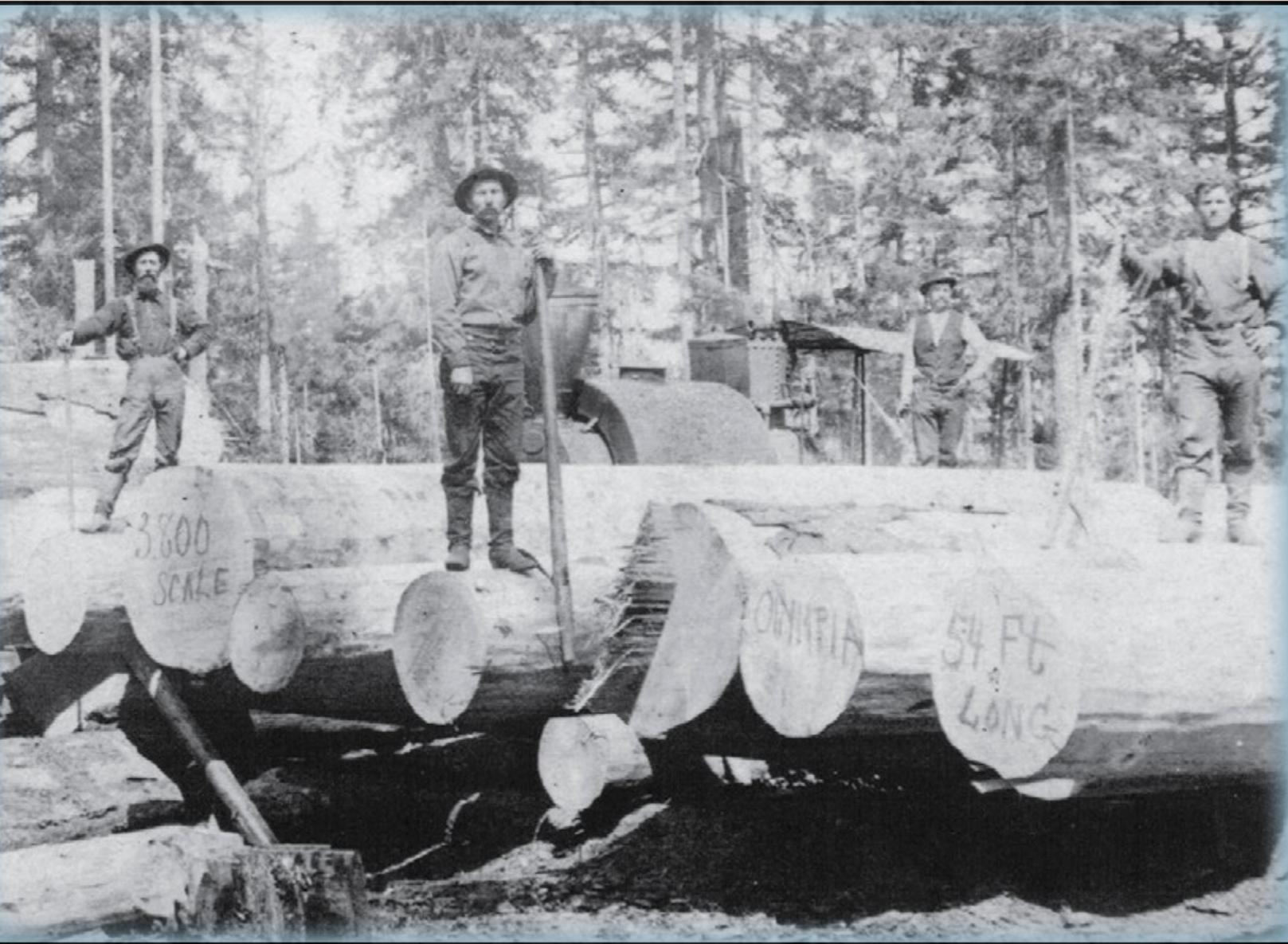
Opposite top: The newest locomotive on the railroad, the OLYMPIA. Photo collection of James S. Hannum, M.D.

Below: The E.N. OUIMETTE at the station in Tenino. The Northern Pacific served the other side of the station. *Photo collection of James S. Hannum, M.D.*



Opposite bottom: McIntosh's logging operation. *Photo, Henderson House Museum, James S. Hannum, M.D. Collection.*





Above: The Morris Brothers logging operation on the South Union branch with the first #2 in the background.
 Photo collection of James S. Hannum, M.D.

(text continued from page 53)

which was just getting started at Mud Bay about four miles west of Olympia.

The last branch of the railroad was operated by Robert B. McIntosh who was engaged in the logging business.

A fourth logger, by the name of George Washington Bush, logged his property adjacent to the railroad. In 1885 he employed 15 men in his logging operation and his daily output was 16,000 board feet of logs. He sold his logs to the Tacoma Mill Company.

In August 1881, the railroad name was changed to the Olympia and Chehalis Valley Railroad Company.

In 1882, the railroad purchased its second #2 which was an 0-4-0T built by

Porter-Bell and Company in 1872 for the Walla Walla and Columbia River Railroad and named WALLULA. She weighed a mere 15,000 pounds, and in January of that year was in the machine shops of the Tacoma Iron Works receiving a complete overhaul. She was expected to soon be operating doing the valuable work of hauling logs. She was, reportedly, involved in a fire at Tenino in 1889 which most likely destroyed her cab.

The last locomotive purchased by the railroad was a new 2-6-0 built by Baldwin in 1884. She was their #3 and named OLYMPIA, had 37-inch drivers, and weighed 38,000 pounds. All of the railroad's locomotives were wood burners.

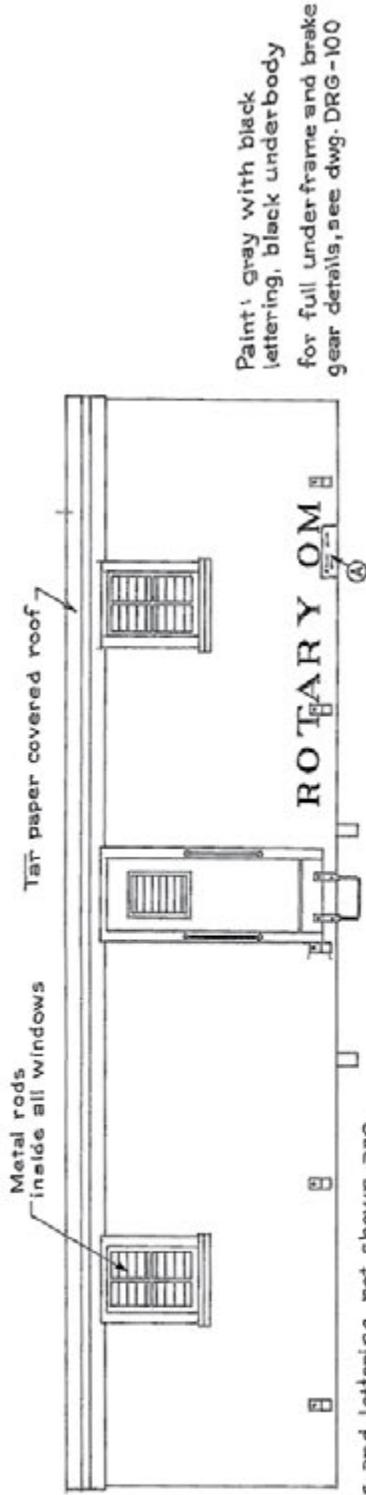
In January 1885, the railroad just finished the construction of a new freight car at their depot, a fact which might be taken as a good business indication. The railroad owned a total of only six freight cars and two passenger cars.

In 1890, the narrow gauge railroad ceased to exist when it was purchased by the Port Townsend Southern Railroad for \$300,000 and then standard gauged, but that's another story. All of O and CV locomotives, except the first #2, went to the Columbia and Puget Sound Railroad, a coal hauling narrow gauge railroad in the Seattle area.

Thanks to James S. Hannum, M.D. for his help in preparing this article. ■

DENVER & RIO GRANDE WESTERN KITCHEN & DINER CAR 04466

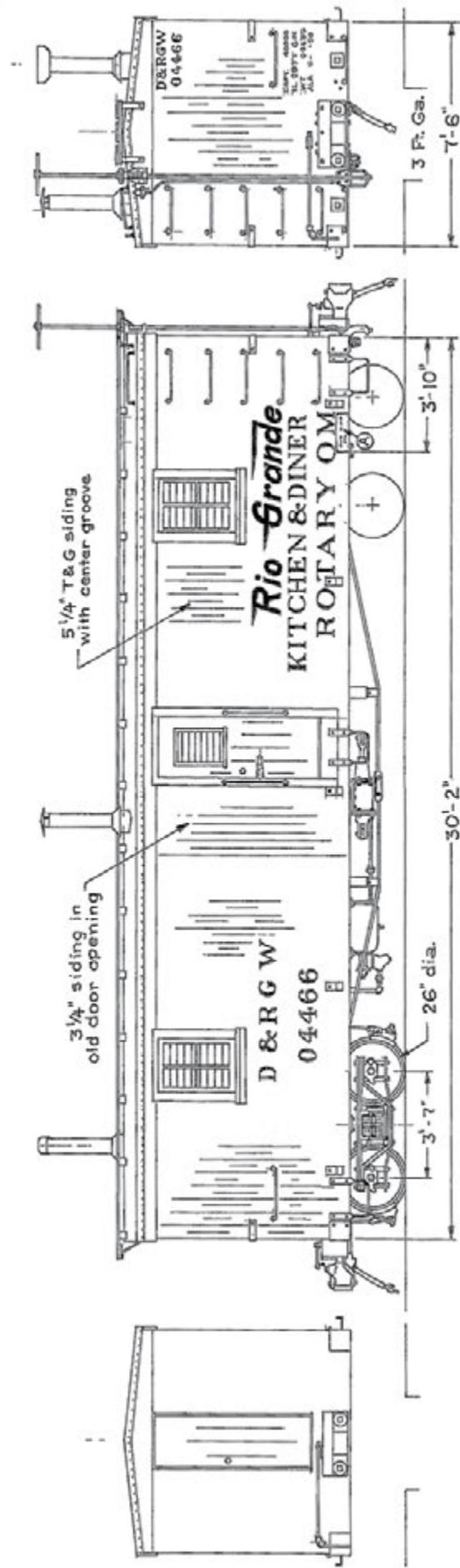
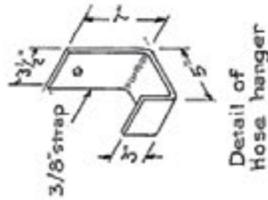
DRAWN BY HERMAN H. DARR 26 Sept. 1983©
SCALE: 3/16 INCH = 1 FOOT



Paint: Gray with black lettering, black underbody for full underframe and brake gear details, see dwg. DRG-100

Note A: Repack data, white lettering on black painted area

Details and lettering not shown are same as on opposite side and end





ADDING LIFE TO A DEAD SPACE



ATMOSPHERE

by Pete Leach, MMR
Photos by the author

Opposite top:

This was the empty space along one side of the RR Wharf on my WW&F layout. The tide is ebbing leaving a small mud flat. But the scene isn't very interesting. The structure above this scene is the Coal Shed used to store coal for the railroad and customers along the line.

Opposite bottom:

Amos Washington readies his dinghy for a day of seine fishing along the mouth of the Sheepscot River in 1925 Maine. Meanwhile, the crew of Portland-built Forney #3 has just delivered a load of sawn lumber to the lower yard. It will be loaded onto a coastal schooner to be used to build wood boxes further down the coast.

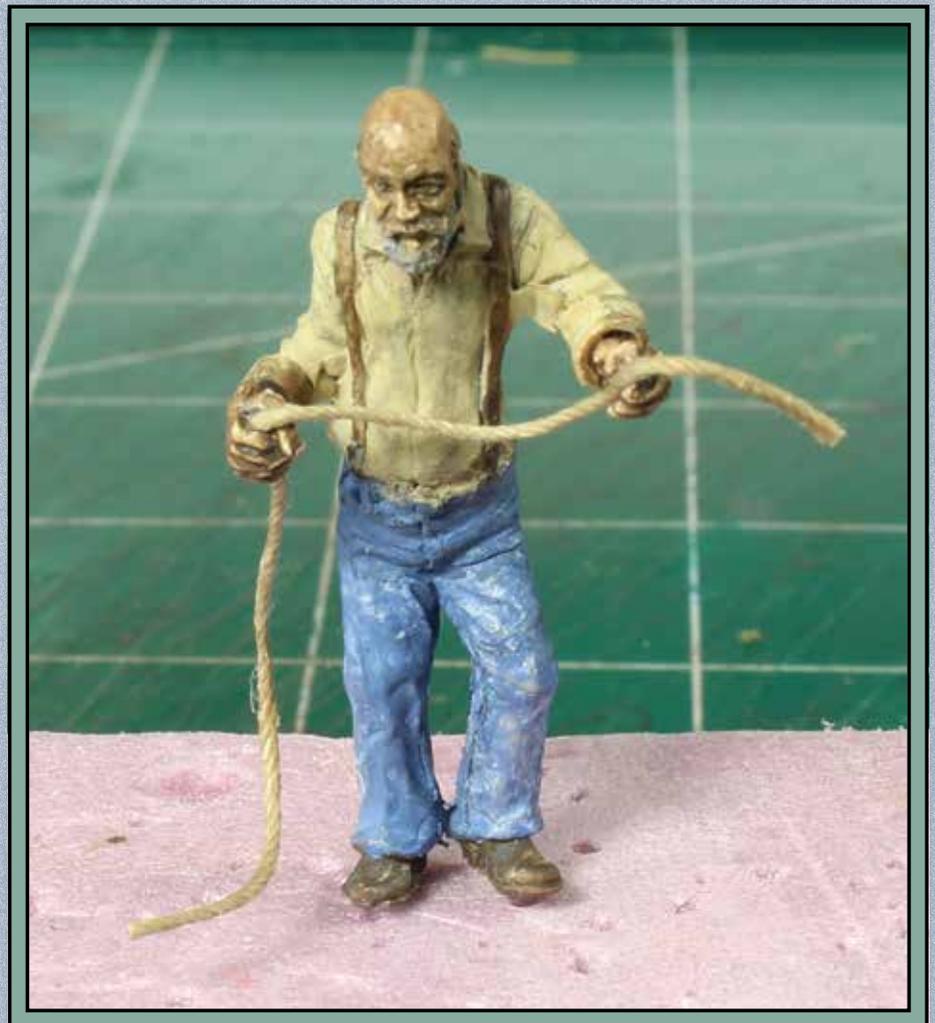
Right: I purchased a Cook figure from Aspen Modeling Company at the National Narrow Gauge Convention in Hickory, North Carolina. Many of their figures come with extra body parts making it easy to make alterations to create a custom figure. I wanted to turn this guy from a Cook into a small-time fisherman working with a small boat. So, I removed the cooking utensil from the Cook's right hand and drilled a hole through his fist for a rope. The left hand came from a different Aspen model. I chose the bald head since the face had so much character. Next Amos received a coat of flat white paint from a spray can. This step will highlight any defects to be corrected with putty.

I painted Amos with various colors of inexpensive craft paints. The rope is a length of heavy line left over from my shipbuilding. I used several coats of brown wash to darken the skin tones to make him an African American. While Maine didn't have a large population of African Americans in the early 20th Century, those who did live there played a key role in the workforce and economy.

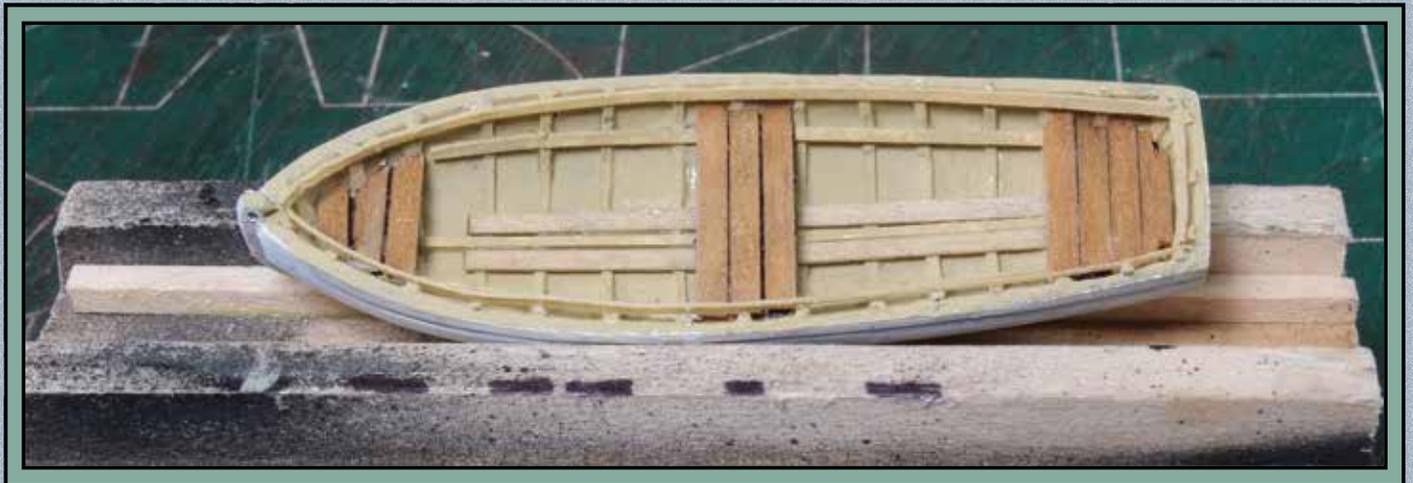
I described my On30 Wiscasset, Waterville & Farmington Railway in the November/December 2020 *GAZETTE*. After 8 years of planning and building my 10- x 12-foot layout, I considered it complete. But we all know a model railroad is never truly finished, is it?

My layout included a scratchbuilt 19- x 19-inch wharf used by the railroad to service freight and passengers transloading between ships and the narrow gauge. Two of the three sides included a scratchbuilt schooner and steam ship. But the third side was dead space with very little detail. Worse, it was one of the first things visible when entering the room. Finding ideas to fill this space was at the top of my list when shopping at the many vendor tables during the 2021 National Narrow Gauge Convention in Hickory, North Carolina.

How I filled this space with a detailed scene is shown in the following photos.



Right: I purchased this Dinghy casting from Crow River Products at the National Narrow Gauge Convention in Hickory, North Carolina. When I got it home, I wasn't sure it was detailed enough for a place in the foreground. The lack of detail on the boat's interior would not add the interest I wanted in my scene. Adding detail to the open boat interior was a lot easier than I thought. I attached a strip of square styrene along the centerline to represent the keel. Next, I used strips of cardstock cut from an old file folder to fashion the vertical ribs. I used some of the same card stock to make the bracing along the sides and as an inner gunwale (the rail along the top.) The thwarts (seats) are strips of 1/32-inch-thick basswood stained and cut to fit. I darkened them with thin CA. The interior was painted a basic tan and the exterior white.



Above and below: The boat details came from my parts bins. The seine net is a small piece of material used for model chain link fencing. I gave it many coats of diluted Hunterline Medium Brown stain. I wet it with several coats of white glue to get it to lay down in a natural position and added a few net floats cut from a round wooden toothpick. The ropes are more of the heavy line forced to conform using the same diluted white glue technique. The oars are from the kit but the tholing pins are tiny bits of wood. (A tholing pin is a wooden rod inserted into the gunwale to act as a fulcrum with rowing, an early form of oar locks.





Above: Here, Amos is waiting for the tide to rise so he can launch his 11-foot Dinghy with his seine net and gear to help him get today's catch. This area is no longer dead space, but an interesting vignette. Projects like this keep me busy on my otherwise "finished" layout.

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U.S. membership: \$30/year
P.O. Box 519, Lansdale, PA 19446
www.anthraciterailroads.org



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www.aclsal.org
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BALTIMORE & OHIO RAILROAD HISTORICAL SOCIETY

Explore the history of America's First Railroad by joining the Baltimore & Ohio Railroad Historical Society. Basic membership levels start at \$45 per year and include four issues of our magazine **The Sentinel**. Other membership levels include a complimentary calendar produced by the society. Annual meetings are held somewhere in the 13 Great States served by the railroad and we hold two mini-conventions — in Ohio in the spring and at our headquarters building in Maryland in the summer. The society also maintains an extensive archive of prototype information, company documents, and library at our headquarters building that is available to all. For more information, visit our website.

U.S. membership \$45/year
www.borhs.org
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BOSTON & MAINE RAILROAD HISTORICAL SOCIETY

Devoted to the history of Boston & Maine, Maine Central, and associated railroads. Members enjoy **B&M Bulletin** three times a year, a bi-monthly print newsletter, calendar, archive, monthly meetings, and cosmetically preserving a B&M 0-6-0 switcher locomotive and combine.

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BURLINGTON ROUTE HISTORICAL SOCIETY

The BRHS is an Illinois non-profit corporation whose members have banded together to share a common interest: the Chicago, Burlington & Quincy Railroad; its two subsidiaries, the Colorado & Southern and the Fort Worth & Denver; and their predecessors. Member publications are **The Zephyr**, a newsletter featuring society news; the **Burlington Bulletin**, featuring articles about operations, equipment and communities; and a yearly calendar. Activities include spring and fall meets. Our growing permanent archive can be found in Baraboo, Wis. For further information, and how to join, visit our website.

U.S. membership: \$40/year
www.burlingtonroute.org
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BYTOWN RAILWAY SOCIETY, INC.

Since its founding in 1969, the Bytown Railway Society has promoted and supported interest in Canadian railways and railway history. Membership in this registered non-profit, all-volunteer society includes six full-color issues of its **Branchline**, Canada's Rail News Magazine. This magazine features articles of historical railway interest and up-to-date news on Canadian railways. In addition to the renowned annual **Canadian Trackside Guide**®, the society regularly publishes books related to the history of Canadian railways and traction systems. The society has monthly meetings, including virtual meetings, and maintains a collection of historic railway vehicles.

U.S. membership \$31/year
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The Canadian National Railways Historical Association (CNRHA) was founded in 1989 (originally known as the CN Lines Special Interest Group) to document the history and operations of the Canadian National family of railways and promote their accurate modeling. Basic membership includes two 48-page, full-color issues of **CN Lines** magazine (published twice yearly). CNRHA also publishes hardcover books, including an award-winning two-volume study of CNR system dieselization. CNRHA focuses on all constituent parts of the Canadian National system during periods of CN ownership, in Canada and the U.S., including steam, diesel, electric, marine, and associated operations.

U.S. membership \$32/two years
Canadian membership \$28/two years
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U.S. membership \$35/year
www.cvrhs.org



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Dedicated to the history of Chicago & Eastern Illinois Railroad. Members enjoy **The Flyer** twice a year, occasional newsletters, social gatherings, an annual convention, photo and data archiving, and more. A museum and archive are maintained in Watseka, Ill. The society also publishes a calendar each year.

U.S. membership: \$30/year
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COTTON BELT RAIL HISTORICAL SOCIETY

Formed to restore SSW 819, the last steam engine built in Arkansas; establish the Arkansas Railroad Museum; and preserve the history of the St. Louis Southwestern. We operate the museum in the building where the 819 was built. Members receive the **Cotton Belt Star** quarterly.

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Formed to preserve the history of the Delaware, Lackawanna & Western Railroad and its predecessors. Members receive **The Roustabout** newsletter four times a year and occasional special publications. A convention is held annually in different locations along the Lackawanna.

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The Denver South Park & Pacific Historical Society is a registered Colorado 501(c)(3) organization established in 1998 to support the preservation of the history and artifacts of all the predecessor lines that became the narrow gauge portion of the Colorado & Southern Railway. The society encourages artifact and equipment acquisitions, as well as dissemination of knowledge about these railroads and their effect on the history of Colorado and the nation. Members receive **The Bogies and the Loop** magazine four times a year.

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www.dspphs.org



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U.S. membership: \$60/year
with the monthly Bulletin sent via email or
\$110/year with the monthly Bulletin sent by regular mail
www.erausa.org
f groups/era1934



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ILLINOIS TRACTION SOCIETY

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www.illinoistractionsociety.org



KANSAS CITY SOUTHERN HISTORICAL SOCIETY

Formed in 1981 to preserve and disseminate history and current events of the Kansas City Southern, and its predecessors. Activities include an annual convention, monthly newsletter, **The Crow**, and a bi-annual journal, **The Belle**, with photos, articles, member stories, and a yearly calendar.

U.S. membership: \$30/year
www.kcshs.org
Not affiliated with the Kansas City Southern Railway Company.



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The KRHS is dedicated to actively preserving and promoting the history and heritage of the Missouri-Kansas-Texas Railroad (known to many as the Katy). For more than 40 years, our members have produced publications and products of interest to historians and modelers alike. These are available through the society. Conferences are held annually to discuss the history and legacy of the Katy, modeling-related topics, and tour historical M-K-T locations. Members receive **The Katy Flyer** four times per year, and the society publishes an annual calendar. Membership information can be found on our website.

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U.S. membership: \$35/year
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MARYLAND AND PENNSYLVANIA RAILROAD HISTORICAL SOCIETY

Formed in 1985, the Maryland and Pennsylvania Railroad Historical Society is dedicated to preserving historical records of the Maryland & Pennsylvania Railroad and predecessors roads. Members receive our **Timetable** magazine four times each year and a calendar for purchase is also published each year. We have an annual meeting each summer.

U.S. membership: \$35/year
445 Richardson Road, York, Pa. 17408-5034
www.maparailroadhist.org



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Preserving the history of the Chicago, Milwaukee, St. Paul & Pacific Railroad (The Milwaukee Road). Members enjoy **The Milwaukee Railroader** four times a year, social gatherings, an annual convention, photo and data archiving, and the society produces a yearly calendar for sale.

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www.mrha.com
f /groups/MilwaukeeRoadHistoricalAssociation



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f @MoPacHistorical



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www.NYCSHS.org www.NYCSHS.net
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www.nprha.org



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U.S. membership \$45/year
www.rgmhs.org
f @rgmhs.org



RUTLAND RAILROAD HISTORICAL SOCIETY

The RRHS was formed in 1987 to collect, preserve, and disseminate information about the Rutland Railroad in Vermont and New York. The subject of Jim Shaughnessy's iconic book, *The Rutland Road*, it discontinued operation in 1961 but a majority of the right-of-way was preserved and is operated by successful modern short lines today. Members receive four issues per year of our journal, the **Newsliner**. Articles cover business history, equipment, physical plant operations, employee recollections, and the importance of the railroad to the municipal and rural areas it served. Prototype modeling is included. A spring annual convention rotates around online communities. U.S. basic membership is \$20/year, other rates apply elsewhere.

U.S. membership: \$20/year
www.rutlandrailroad.org



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1205 S. Air Depot # 101
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P.O. Box 22941, Chattanooga, TN 37422
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f @Southern.Railway.Historical.Association



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WESTERN MARYLAND RAILWAY HISTORICAL SOCIETY

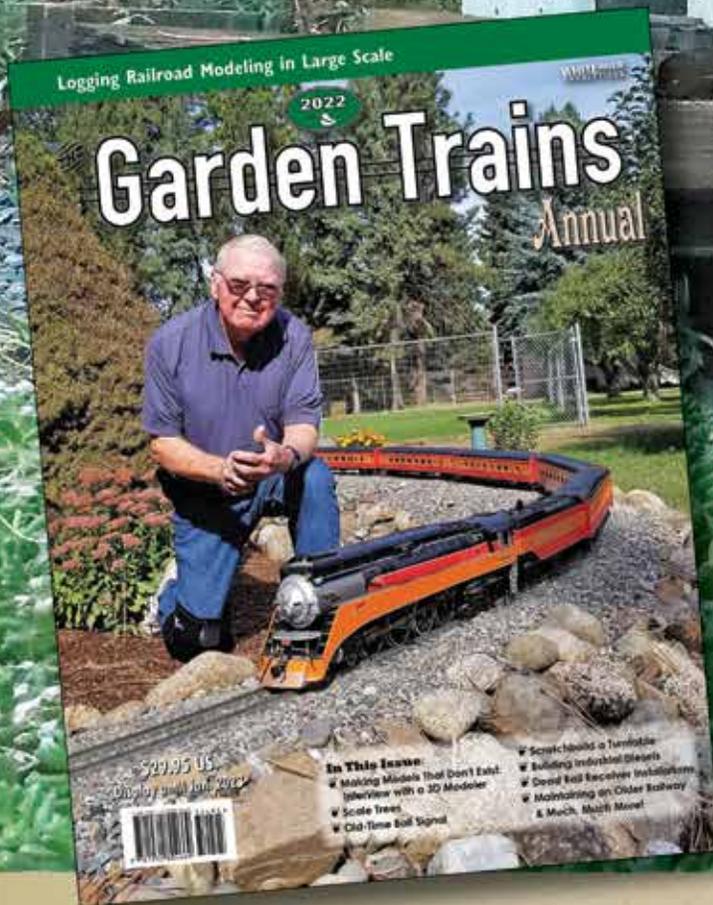
The Western Maryland Railway Historical Society was founded in 1967 for the purpose of preserving a portion of America's rich railroad history. The society is one of the few organizations of its kind to own and operate its own museum in ex-WM buildings. It serves as a focal point of our society activities and events. This affords members and the public alike the unique opportunity to enjoy and study our collection to its best advantage. We provide programs for community organizations; publish a quarterly magazine, the **Blue Mountain Express**; hold annual conventions; and do special runs of WM model cars.

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THE U.S. BAKERY • BODIE, CALIFORNIA

DRAWN BY NEIL A. PFAFMAN
HO SCALE

In the early 1890s where would one get a cake, pie, or even a loaf of bread in Bodie, California? At that time the population was mainly men living in dugouts, rooming houses or hotels who did not have the know-how or means to cook, let alone bake. Baked items would be found either in a bakery or in some one's home. Fortunately, there were several bakeries snuggled in between saloons in Bodie.

One of them was the U.S. Bakery operated by Mrs. James Perry who also sold groceries and ran a restaurant.

On July 3, 1892, a small fire started in her bakery, but was discovered and quickly put out with buckets of water. Several people noticed a lingering smell of coal oil after the fire.

On July 26, 1892. A Chinese cook arriving for work at 2:00 a.m. discovered a fire and noticed the smell of coal oil. The alarm went out and the local

volunteer firefighters arrived with a hose cart and ladder wagon. Fortunately, there was a fire hydrant nearby and someone had turned on the water at the Standard Mill. Just as they were getting ready to put out the fire, someone turned the water off.

The fireman sent someone to turn the water back on and began hauling items out of the building, but the fire had spread to several nearby buildings.

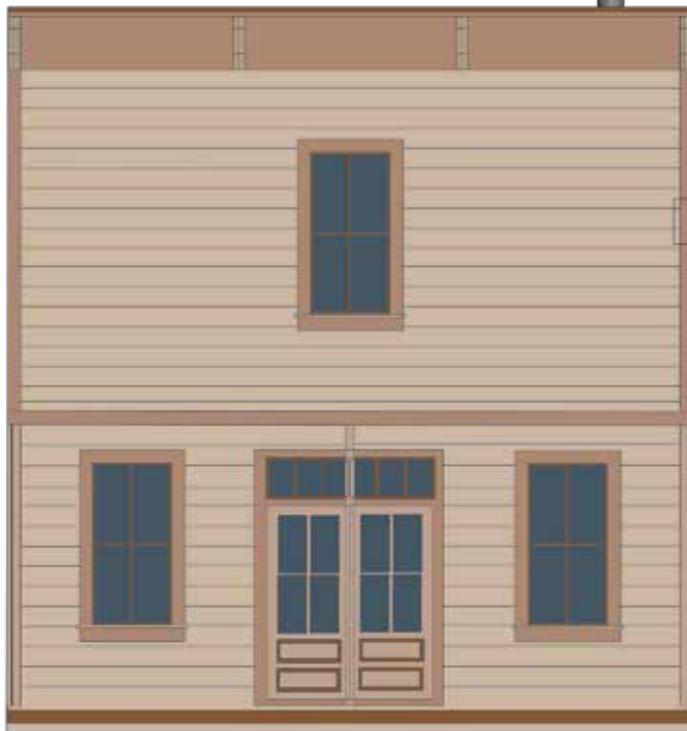
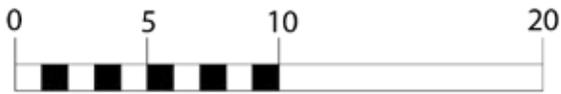
By the time the fire had been put out, half the buildings from Bodie's boom times were smoldering ruins and the street was littered with everything from a piano to a paper full of pins.

Fortunately, no deaths were recorded, but many citizens were left with no place to sleep or eat. Many of Bodie's homes were opened to feed and bed those who were lucky to survive with not much more than the clothes on their backs.

Arson? No one was ever charged.

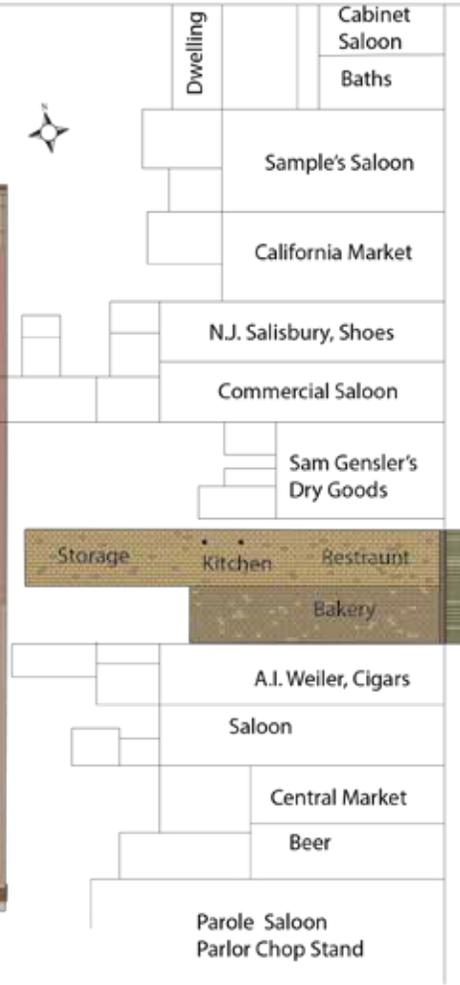


DRAWN BY NEIL A. PFAFMAN
HO SCALE



EAST ELEVATION

UNION STREET



All of these buildings burned to the ground on July 26, 1892 by a fire that started in the U. S. Bakery.

U. S. Bakery

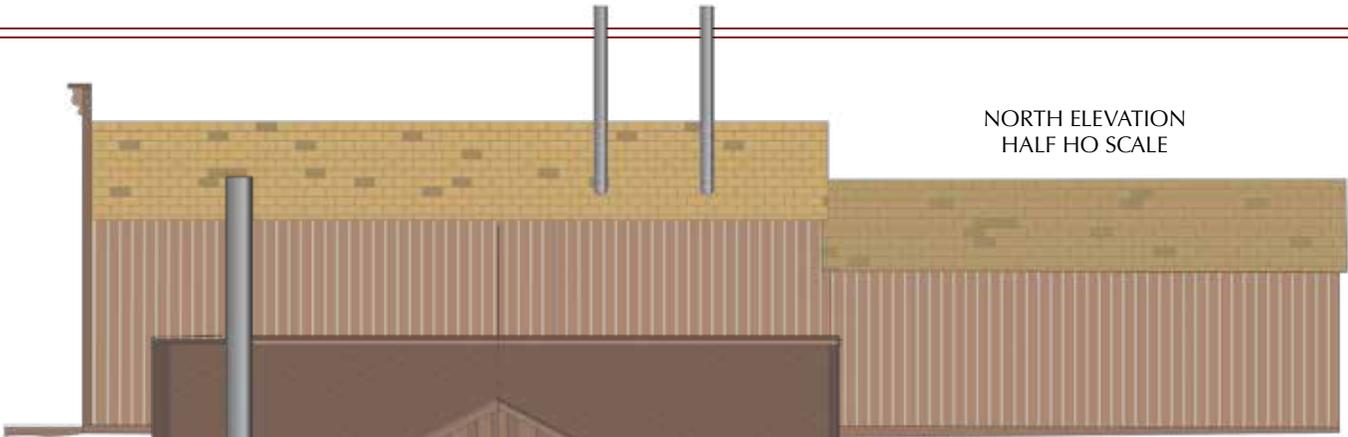
MAIN STREET

THE U.S. BAKERY • BODIE, CALIFORNIA



SOUTH ELEVATION

NORTH ELEVATION
HALF HO SCALE

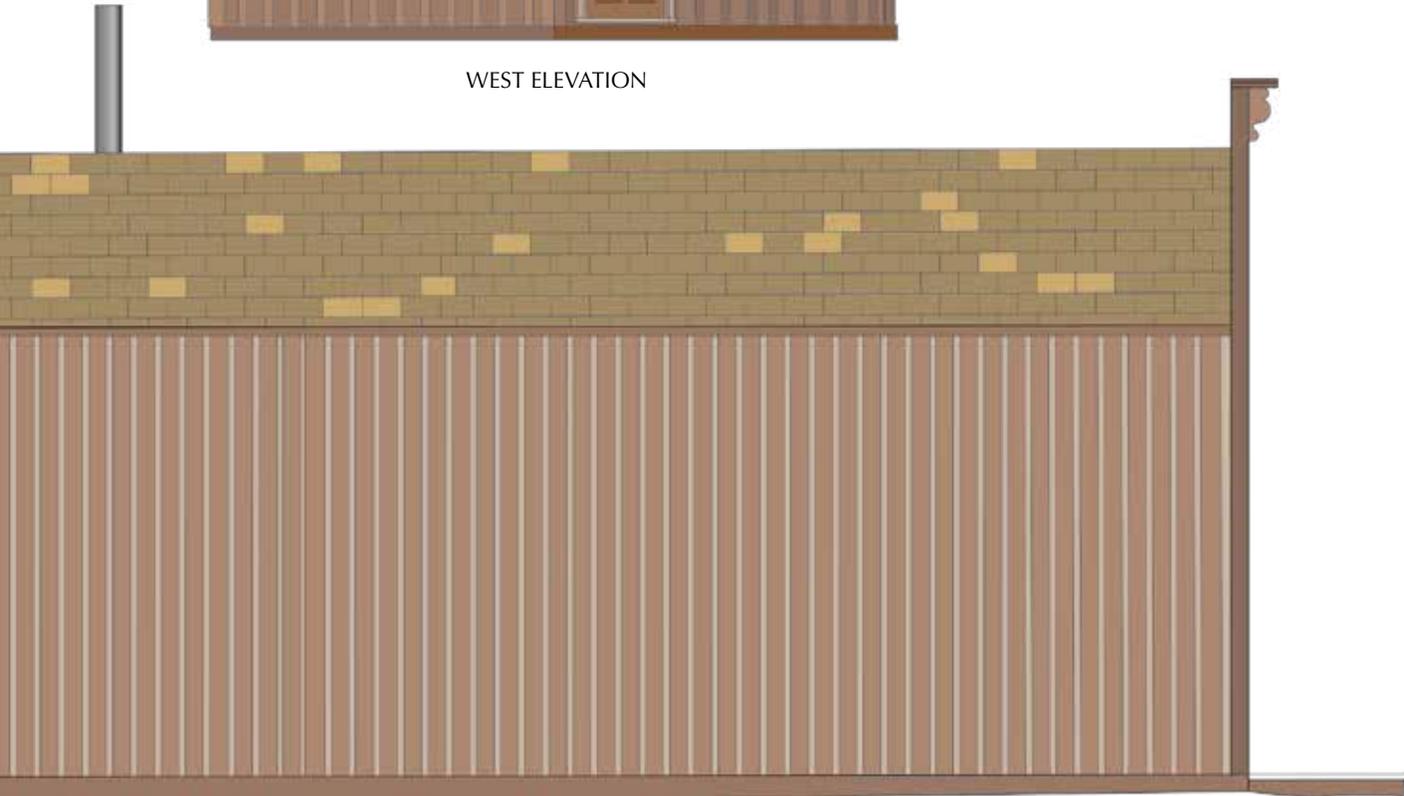


THE U.S. BAKERY

BODIE, CALIFORNIA

DRAWN BY NEIL A. PFAFMAN
HO SCALE

WEST ELEVATION



THIS WALL WAS SHARED WITH THE CENTRAL MARKET OR A.I. WEILER, CIGARS

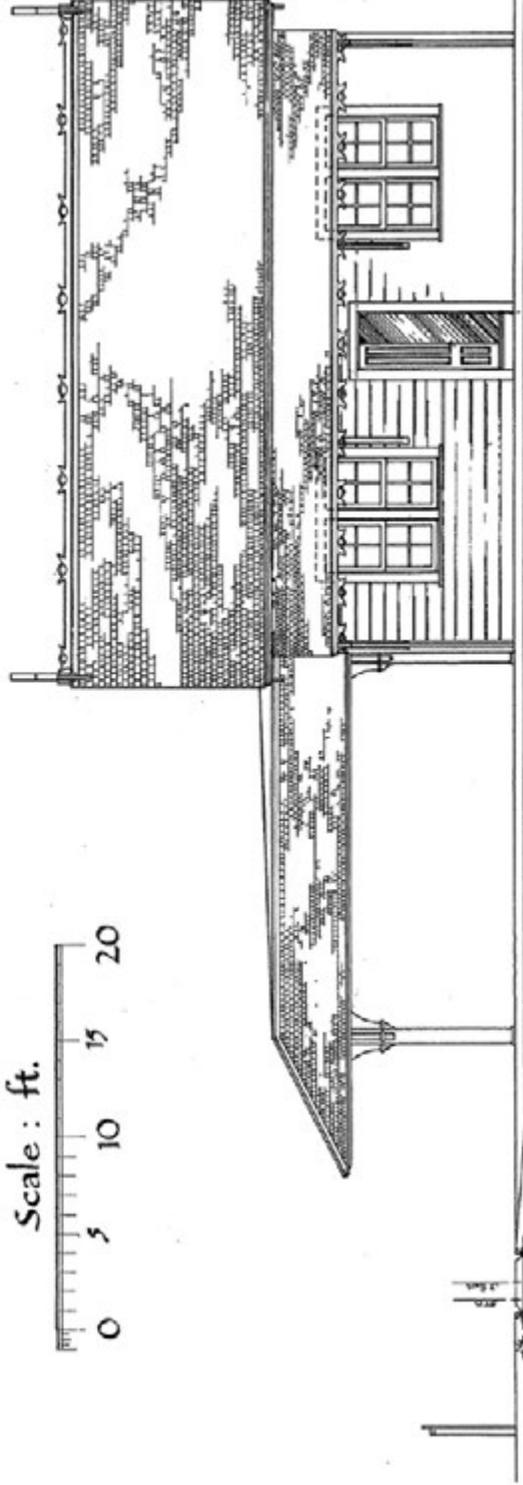
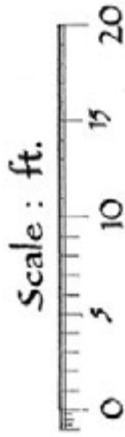
South Pacific Coast Railroad

Passenger/Express Depot

based on

Franklin St. Terminal

DRAWN BY GARY CAVIGLIA
SCALE: 1/10 INCH = 1 FOOT



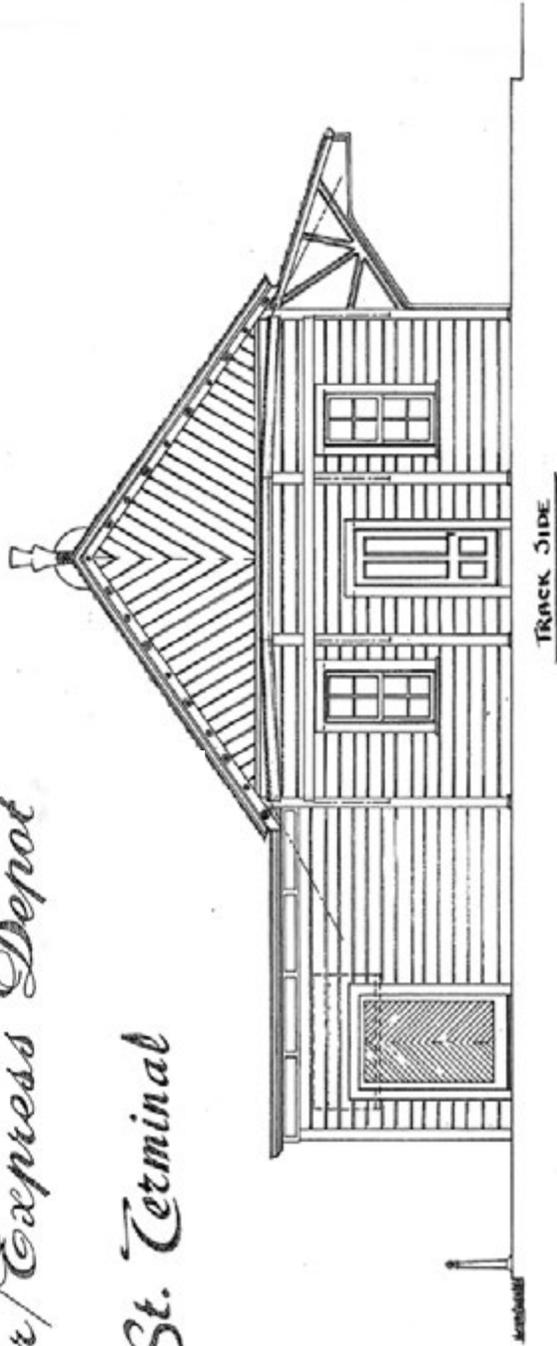
FRANKLIN STREET (FRONT)

South Pacific Coast Railroad

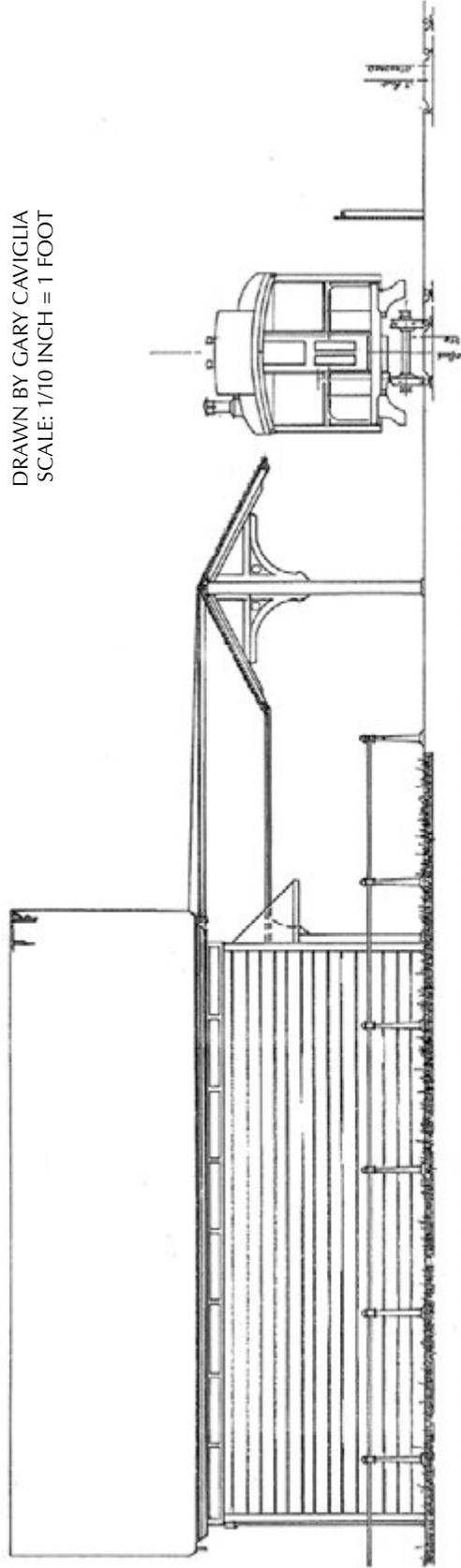
Passenger/Express Depot

based on

Franklin St. Terminal



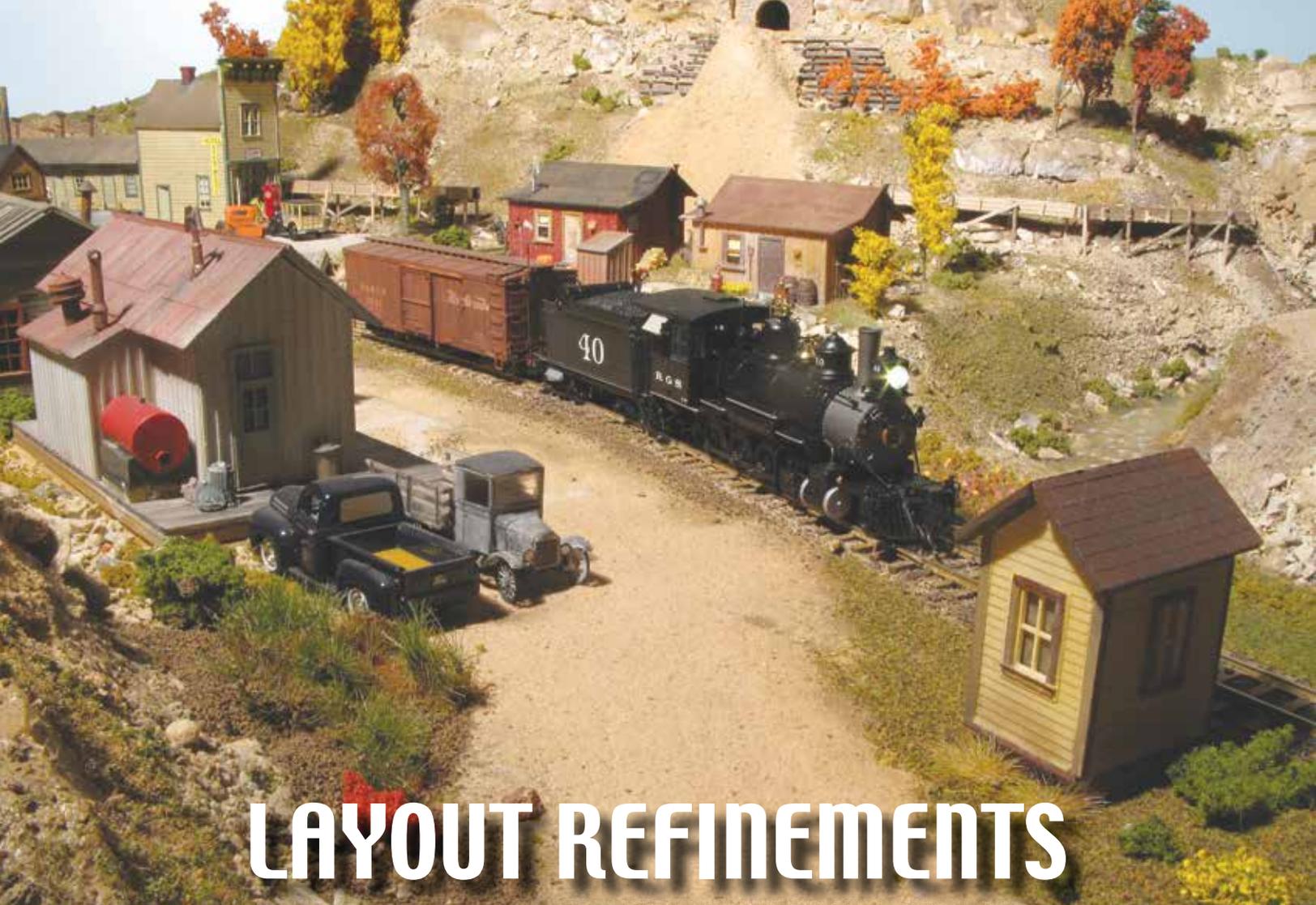
TRACK SIDE



BACK SIDE

DRAWN BY GARY CAVIGLIA
SCALE: 1/10 INCH = 1 FOOT





LAYOUT REFINEMENTS

BUILD SCENES OFF-LAYOUT

by Dr. Gregg Condon, MMR
Photos by the author

One of the most striking layout refinements is to add scenery. Many layouts have areas where no scenery has been started. Even if scenery is complete, a longtime hobby can be made out of replacing the proverbial weak link with a new best-effort.

One of the biggest stumbling blocks in the way of working on scenery is the mess. Working on scenery entails tools, a variety of glues, and dozens of types of scenic materials. All this can shut down a model railroad for the duration of the project. But wait — scenery doesn't have to be built on the layout!

USE A DEDICATED SCENE SHOP

Large portions of model railroad scenery can best be built off the layout. In planning my train room, I reserved some space for a crew lounge and other space

for a workshop. My workshop includes a table I built as a temporary scene shop. When the layout was completed, the table came down and the space is now used for other things. Even if you don't have a separate dedicated area for a scene shop, one area of the layout benchwork could be reserved for that function and finished last. Equally reasonable would be to have a worktable in the garage. While I have a dedicated scene shop near my train room, which I have used extensively, I also have an outdoor workbench where I have done as much scenery construction as possible when weather permitted.

Title photo: Detailed scenes like this one can be better assembled off the layout and then blended in.

Scenery at track level pretty much has to be finished in place. Almost everything else can be built at a remote location. About 75 percent of my RGS scenery was built off-layout. My dedicated scene shop is efficient because I take my work to the tools. It keeps the mess out of the train room and lets me run trains through the construction zone. With only one limited area of scenery under my nose at a time, best efforts are promoted.

ENJOY THE EFFICIENCIES

Follow along with the photos and see efficiencies that can be enjoyed. While my methods almost always entail layered insulation foam and cloth-shell, scenes can be built remotely on plywood with traditional plaster methods.



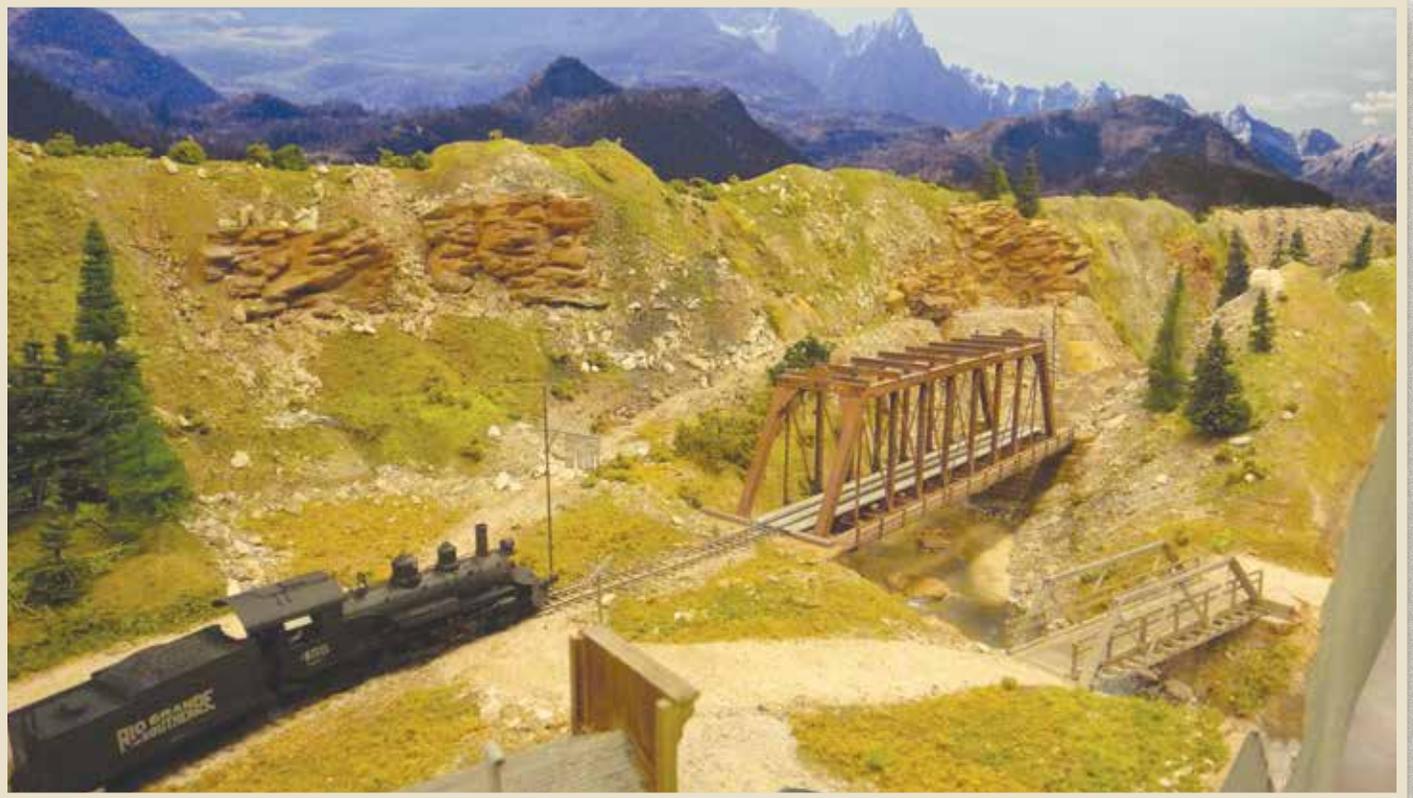
Above: The author's scene shop with a piece of foam scenery under construction at right. Here are tools, a variety of glues, and about 60 varieties of scenic ground cover.



Left: Here is the outdoor workbench disguised as a decorative fence. It's a favored work station in good weather, because Nature sweeps the floor!

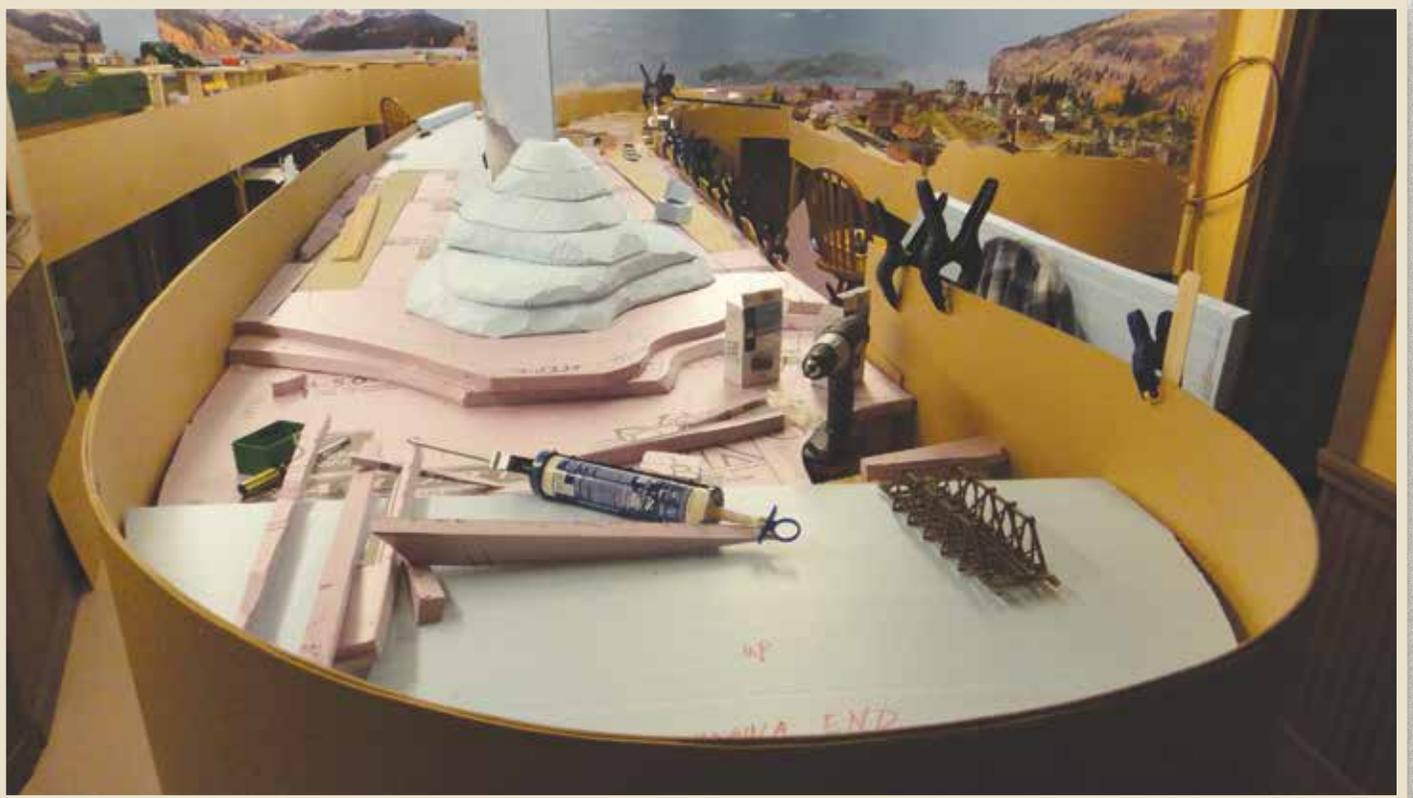


Below: Here, under construction, is one of several pieces of mountainside for Burns Canyon.



Above: The piece of scenery behind the bridge has been blended end-to-end with similar pieces that were finished outdoors.

Below: Layered foam has been stacked for a test-fit at the end of a backdrop.





Above: Work is in progress with some rock castings made from rubber molds, vertical rock strata cast in accordion-folded aluminum foil, some cloth-shell patches, and some crevices filled with patio block sand which contains concrete and hardens when sprayed with water.



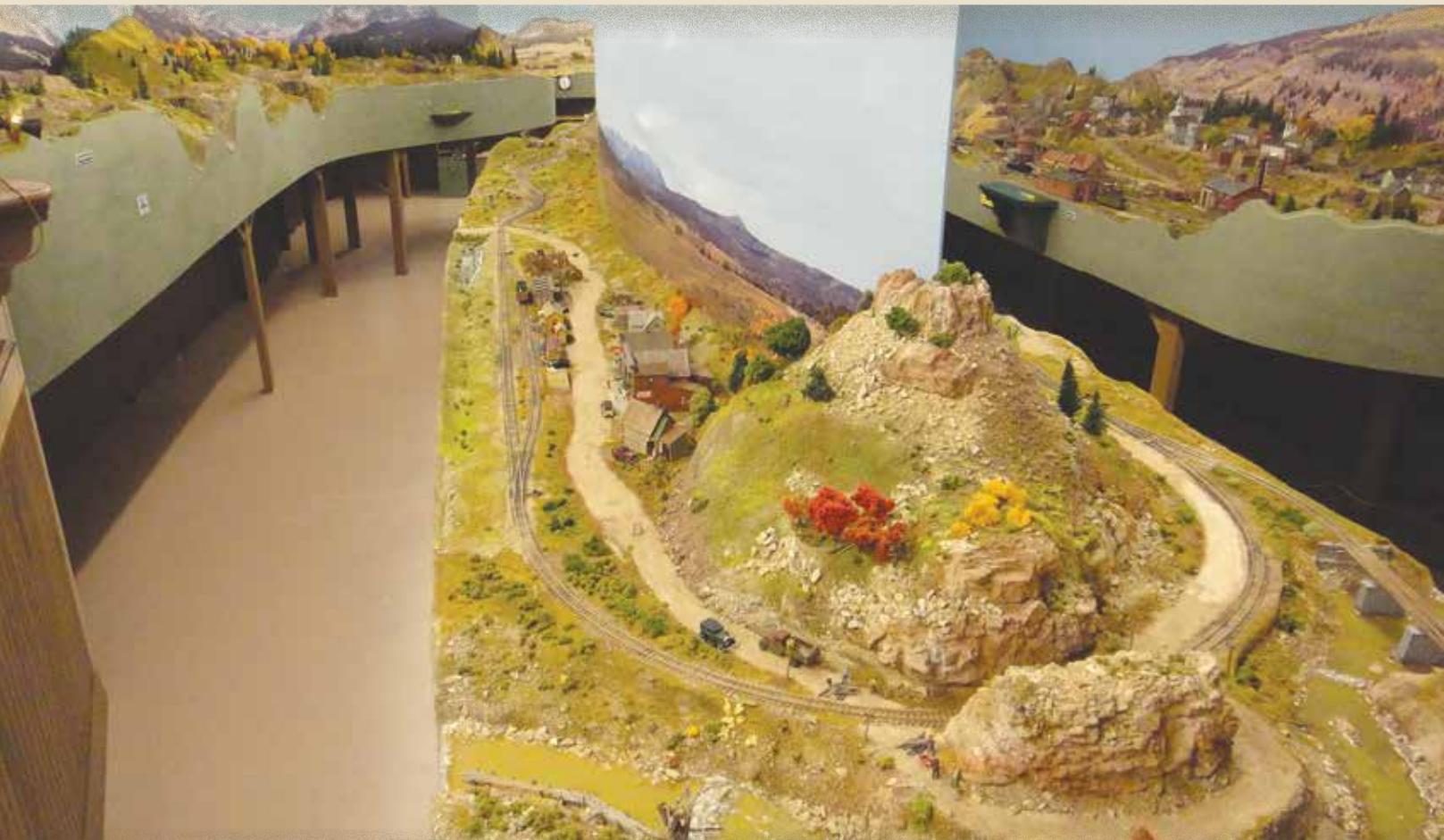
Above: Here are more rock castings and paving cement.

Right: The mountain has been painted an earth tone.



Below: Ground cover has been applied and it's beginning to resemble Colorado!

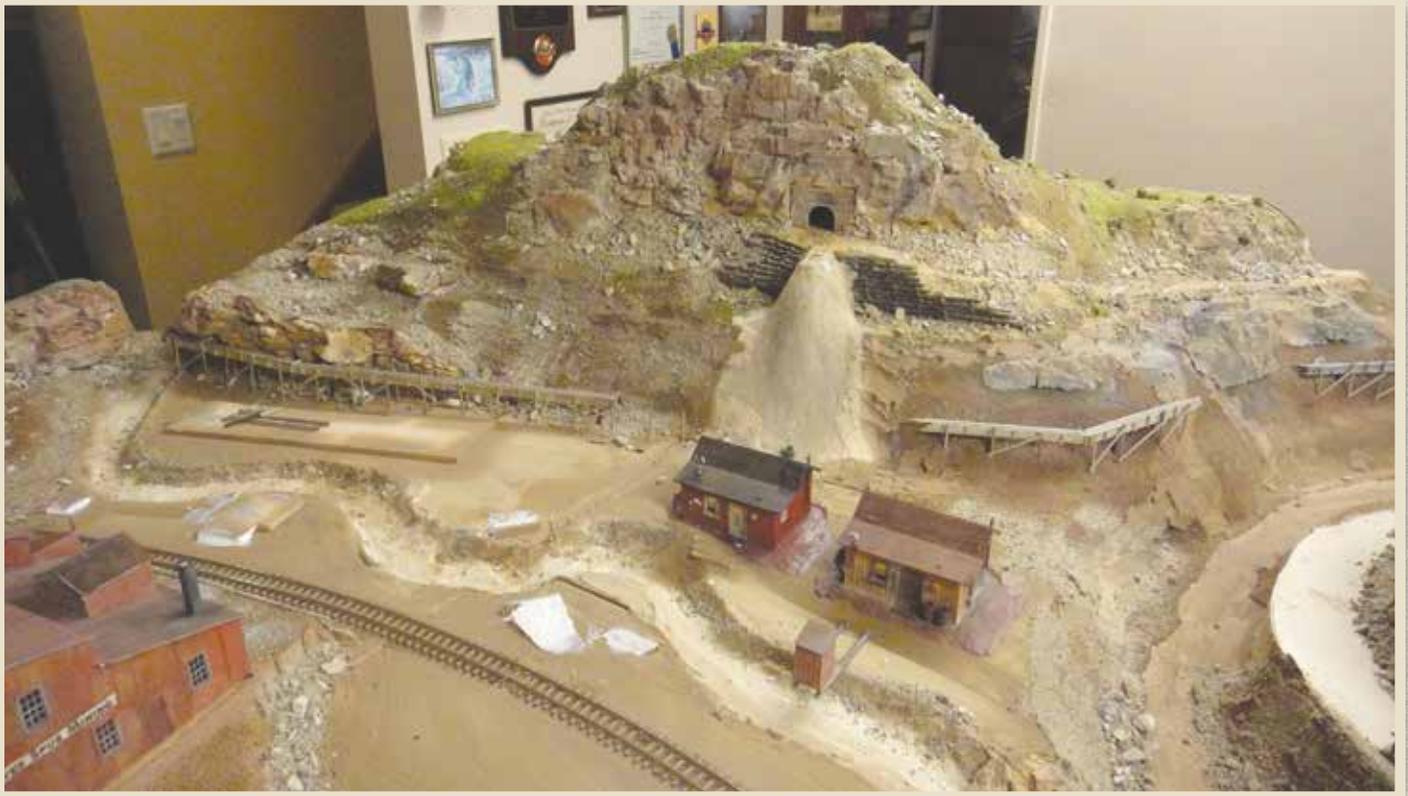




Above: The mountain is in place and blended with adjoining scenery.

Below: Here is a mountainside with mine adit being built outdoors.





Above: The finished mountainside is being blended into the overall layout scene. 🚂

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THE ROMLEY TURNTABLE ON MY On3 COLORADO CENTRAL & SOUTHERN



PART 2: THE PIT

by Dan Windolph
Photos by the author

Romley, Colorado, was essentially an ore loading facility for the Mary Murphy Mine, located between St. Elmo and Hancock. This was an isolated section of the C&S due to abandonments, and the turntable was used to turn locomotives for the return trip back down to Buena Vista.

The turntable was located just off the main line on the edge of a mountain. This was a dangerous location, as engineer Sam Speas found out when the iron center casting on the turntable broke, causing the table to tilt, allowing locomotive #67 to roll off and plunge 30 feet down the mountainside. Luckily, Speas jumped and landed in the pit, suffering only minor bruises.

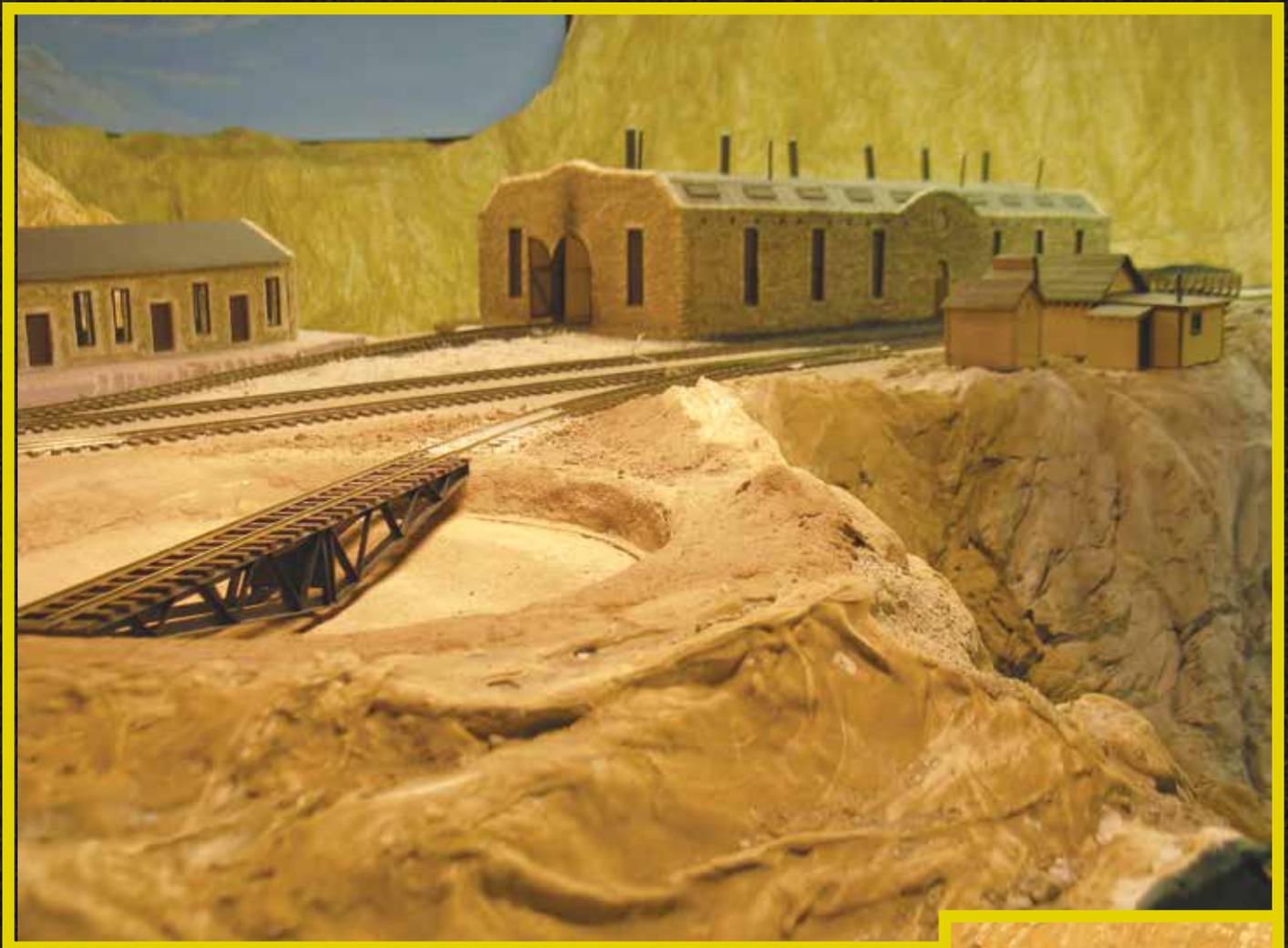
I located the turntable on the highest point on my layout, next to the Alpine Tunnel engine house, which is just above the Chalk Cliffs. The cliffs seemed like a perfect place to represent the hazardous turntable location at Romley.

I installed the basic plywood pit in the benchwork in 1999 and hadn't looked at it since. I was unhappy to discover that the base had shifted during the years and was no longer level. This meant I had to shim one side of the bronze bearing to bring it level again. I hadn't intended to make the ring rail functional which was good, as the base was no longer level.

Once the center bearing was leveled and glued in place, I could then align the

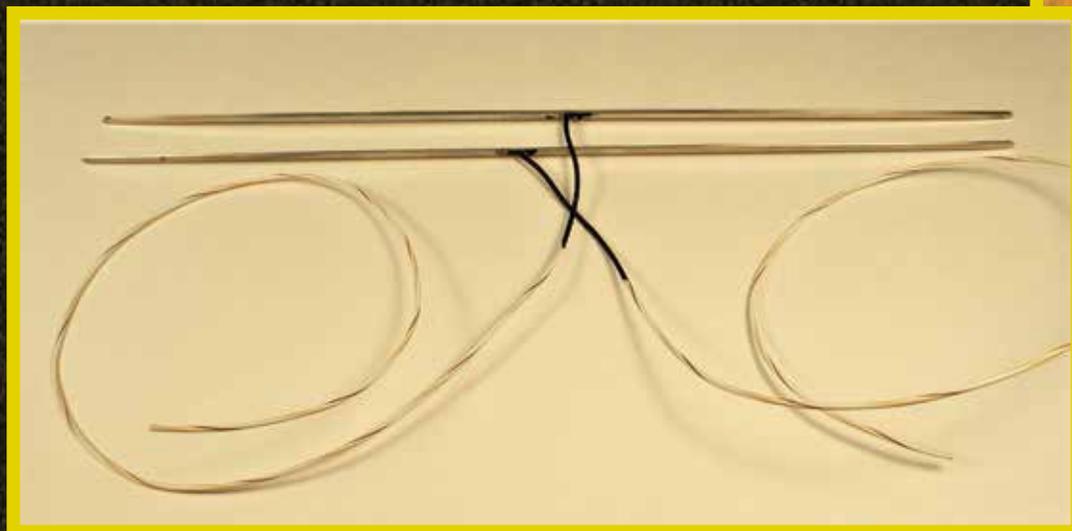
center pivot shaft on the bridge. Instead of a steel rod for the center pivot, I used $\frac{5}{16}$ -inch brass tubing because it wouldn't take as much heat to solder it to the bridge. I ground flat spots on the top of the tube and soldered it between the center cross pieces of the bridge. Instead of using the ring rail for current pickup, I soldered wires to the underside of the rails and ran them down the tube. This wouldn't work for a conventional roundhouse turntable because the wires would twist as it turned, but it wasn't a problem as this turntable only goes back and forth and not in a circle.

(text continued on page 81)



Above: The turntable located next to the massive Alpine Tunnel engine house and stone boarding house. I assembled both structures from South Park kits once produced by George Sebastian-Coleman. The depot and outhouse were built by my wife, Pat. The top of the Chalk Cliffs can be seen below the depot.

Right: Bronze oil-filled bearing in the bottom of the pit has been leveled with shims and glued in place.



Left: Wires to provide current pickup have been soldered to the bottom of the rails.



Above: Completed bridge with wires from the rails coming out of the hollow brass pivot tube.



Above: An additional bronze bearing has been placed on the bridge facing down. The flat flange will ride on the flange of the pit bearing.



Right: As described in the text, this is an example of how I used a collar to eliminate side play when the bridge was in the pit.



Left: The completed turntable installation has a basic ground cover of sanded grout.



Above: This photo shows the turntable at the edge of the Chalk Cliffs. More ground cover will be needed later.

(text continued from page 78)

Now that the bridge was complete, I painted it and glued ties and rail in place. I had another bronze bearing, so I slipped it onto the tube up against the bottom of the bridge just to see how many shims I would need to bring the rails level with the track to the pit when I placed the bridge in the pit. I was amazed to see that everything lined up perfectly without shims. I wish I could say this was the result of careful planning, but it wasn't. This proves once again, that sometimes it's better to be lucky than good.

I noticed I had gotten some overspray on the pivot tube when I painted the bridge, causing a slight bind when I rotated the bridge in the pit. I used a rubber wheel in my motor tool to polish off the excess paint and relieve the bind. I was a little too aggressive and again removed some of the tube itself, causing some side play when the table was in position. I didn't

want to solder a new tube in the bridge because it was already painted, so I tried sliding a collar with a set screw on the tube up against the plywood base; this worked fine to eliminate the movement.

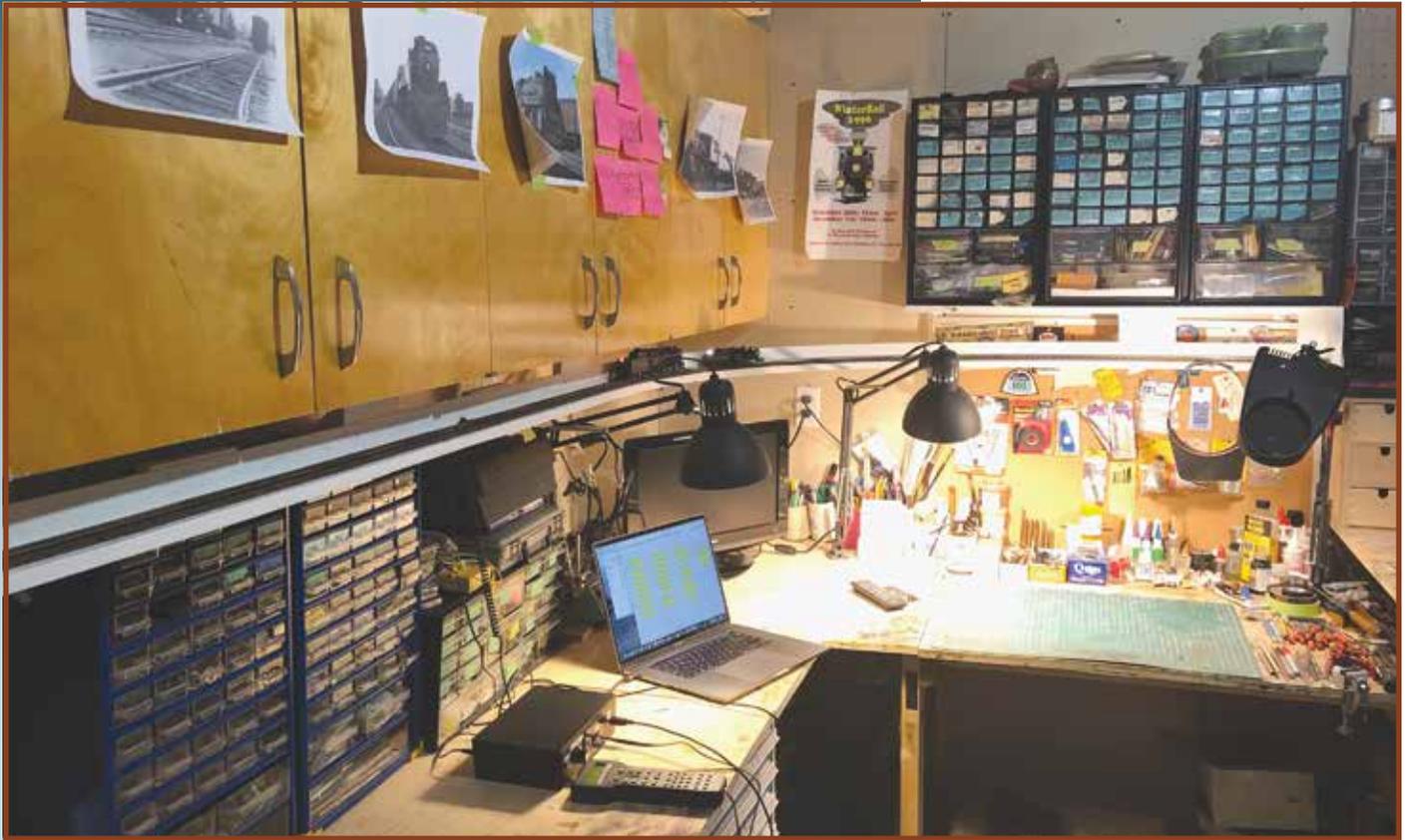
Ground cover in and around the pit was done with Polyblend Sanded Grout. I started using this product some time ago after *GAZETTE* editor, Bob Brown, mentioned it in one of his editorials. It comes in different colors, so I bought shades I thought would look good as ground cover. It works well as a base for adding other scenery materials, but also looks fine by itself as basic ground cover. I intend to add more material around the pit to help blend it into the surrounding Chalk Cliff area.

I like having the turntable at the edge of the cliffs, but at 69-inches above the floor, it's an awkward location to work on while standing on a ladder. Because

of this, I probably won't operate it very often, but I never intended for it to get much use. Rather, it was primarily a unique brass building project I thought would be interesting, which it was, once I finished all the angles. It isn't powered, so I turn it by hand while standing on a ladder, another reason it will see little use.

Photos of the turntable are scarce, but the best for the modeler are found in *C&S Highline Memories & Then Some* and *South Park's Gunnison Division & Then Some*, by Tom and Denise Klinger. Photos in these books clearly show construction details not found in other books.

I didn't intend for this project to take more than 20 years to complete, but now I can finally finish scenery around my Alpine Tunnel area—possibly the subject of a future article. ■



A LOCOMOTIVE TEST TRACK DOING IT IN STYLE

by Craig Symington, MMR
Photos by the author

Title photo: This is how my workbench looks with the test track installed under my cabinets and shelf. You can see the white test track in the middle of the photo with two locomotives on it. It turned out to be unobtrusive as planned. For now, I have the junction box sitting on my workbench. In the future I may mount it under the workbench or under the test track to get it out of the way. For now, I'm not sure what will work best. What I am sure about is how pleased I am with this system and how well it works. It's been a complete success.

One of the aspects of this hobby that I really enjoy is the challenge of reworking brass locomotives. However, after rebuilding the drivetrains and adding DCC, I've never had a very good way of test running these locomotives. The best I've been able to come up with is a jumble of wires and a length of flex track that I temporarily set up on my workbench or coffee table. For the longest time, I've wanted to build a proper test track. After years of struggling with this less than adequate setup, I developed a list of criteria that any future test track would need to meet:

1. When I'm rebuilding a brass locomotive, one of the first steps I take is to make sure that the drivetrain runs smoothly. Often, I'll upgrade the motor and sometimes I'll replace the gearbox. Once this is done, I will use a DC power pack to ensure that the mechanism is running smoothly. My new test track was going to have to support DC operation.
2. When there is a bind in the mechanism or I'm trying to find a short, I will place the locomotive in a foam cradle upside down with test leads clipped to the model. Using another test lead,

I will probe possible problem areas to find shorts. My new setup was going to need the ability to support test leads.

3. Once the locomotive that I'm working on runs well under DC power, I'll install a DCC Decoder. I then retest the locomotive performance to ensure the DCC Decoder and locomotive are working at peak performance. Having the ability to run the model under DCC power was going to be a must for this new test track.
4. I use a dedicated NCE Power Cab, NCE USB interface, JMRI and my computer to program Decoders. The new test track was going to need to support these devices without having a jumble of wires all over my workbench.
5. Test running on a three-foot length of flextrack never seems to be long enough, but there isn't enough room on my workbench for anything longer. The new test track was going to have to be at least two lengths of flextrack long and not clutter my workbench. Preferably, it would also have a curve in it to test for shorts as the wheelsets slide from one side to another.

I wasn't sure how I was going to meet these objectives until I saw a Facebook posting from a fellow modeler. He built a test track on legs that floated above his workbench like a skinny table. With this setup, he still had much of his workbench free for working on engines. This gave me an idea. Above my workbench are cupboards. I realized that I could take this raised test track idea and flip it upside down. My new test track would hang under the cupboards!

I wanted my test track to be pleasing and strong, so I decided to build it out of pine. The roadbed was made from laminated strips of pine like the roadbed I created for my Telluride branch (May/June 2014 GAZETTE). I made pine blocks for hanging the roadbed from the cupboards. Along the back of the test track, I added strips of Masonite so that a derailed locomotive couldn't fall off the back and come crashing down on my workbench.

I test fit my test track in place prior to painting. Once I was satisfied with the fit, I painted everything with white paint. As much as I write about HO_n3 modeling, I also have a sizable collection of Canadian Prototype steam engines that I enjoy working on. Because of that, I installed HO cork and HO/HO_n3 Micro Engineering flextrack on my test track. This

would allow me to enjoy both facets of the hobby.

To solve the "jumble of wires" issue, and the fact that some of the NCE components were simply electronic boards that need electrical short protection, I designed a junction box to house the components and provide simple plugs for connecting the wires. Through this box, I would plug in the NCE throttle and DC power pack. The box would also have outputs for the test track and test leads to clip to a locomotive. Following an internet tip from Phil Floyd, I also added a Decoder who's grey and orange motor wires could be set as an output to run a non-DCC equipped (DC) locomotive using the NCE DCC controller. After sketching a wiring diagram, I ordered all the non-DCC components from Digi-Key.

Please follow along with the accompanying photos as I walk you through this project and provide more details. In the end, my junction box is slick and met all my requirements. The test track is unobtrusive and works well. The whole setup far exceeded my expectations and I'm really pleased with the results. It's made working on, and programming locomotives a lot easier. I hope my experience inspires you to create your own test track.

PARTS LIST

Digi-Key Part Number	Description	Quantity
HM1048-ND	BOX ABS BLACK 8.701"L x 5.906"W	1
679-3545-ND	KNOB FLUTED W/SKIRT 0.250" PLAST	1
CKN10619-ND	SWITCH ROTARY 3POS 2.5A 125V	1
1528-2339-ND	BLACK NYLON SCREW AND STAND-OFF	1
839-1449-ND	CONN PWR JACK 2.5x5.5MM PNL MNT	1
839-1165-ND	CBL ASSY M STR 2.5MM 6' 24AWG	2
CP-2208-ND	CABLE ASSY R/A 3.5MM STEREO 6'	3

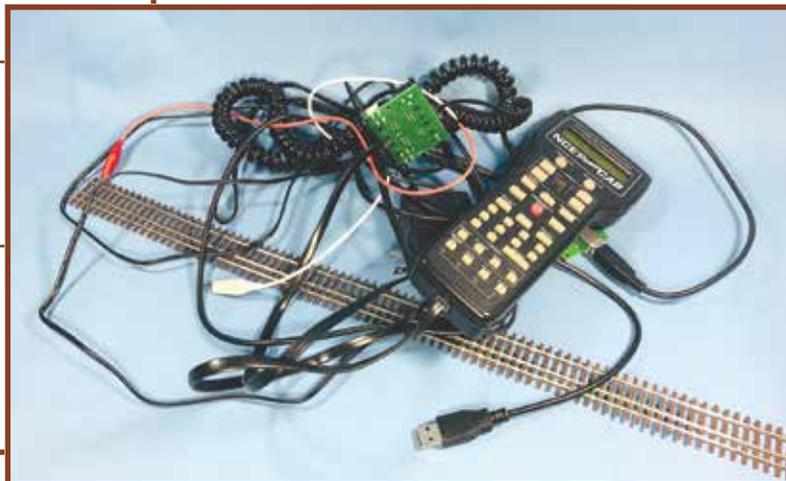
DCC Components:

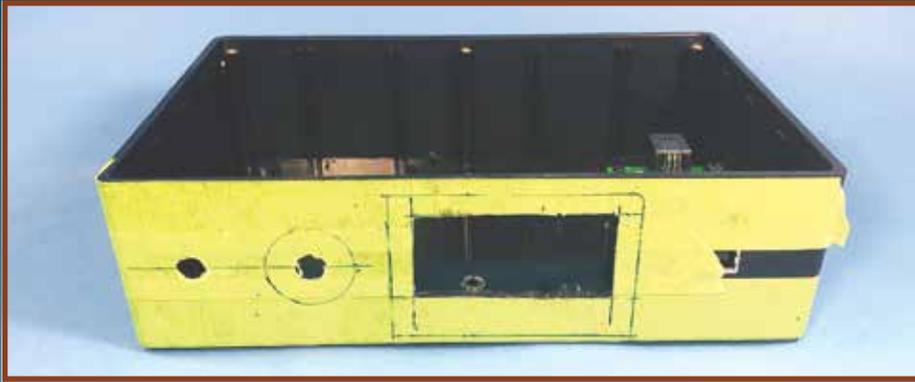
NCE Power Cab Starter Set
 NCE USB Interface for Power Cab
 TCS T1 Decoder

Miscellaneous Parts:

Micro Engineering Code 55 HO/HO_n3 flextrack
 Crimp on electrical spade connectors
 Alligator clips
 Heat shrink tubing
 Solder

Below: This jumble of wires is what I have been using for the past few years to test locomotives running under DCC. In the mess, is a USB board for connecting the NCE Power Cab to my computer for using JMRI Decoder Pro to program locomotives. The NCE components have un-protected circuit boards, and it was a matter of time before a stray wire shorted something out and destroyed the components. The single piece of flextrack was never long enough and would slide around on my workbench as the wires pulled on it. This mess was the inspiration for this project.





Left: I added masking tape to the outside of the junction box and planned where all the components would go. I wanted the component placement to make sense from a flow standpoint. All the inputs and controls are on the front of the box and all the outputs are on the back of the box. This also allowed for a logical and organized flow of the wires inside the box. I used various drill bits and a utility knife to cut out the openings.



Left: The front of the junction box has plugs for all the inputs, and controls what is output.

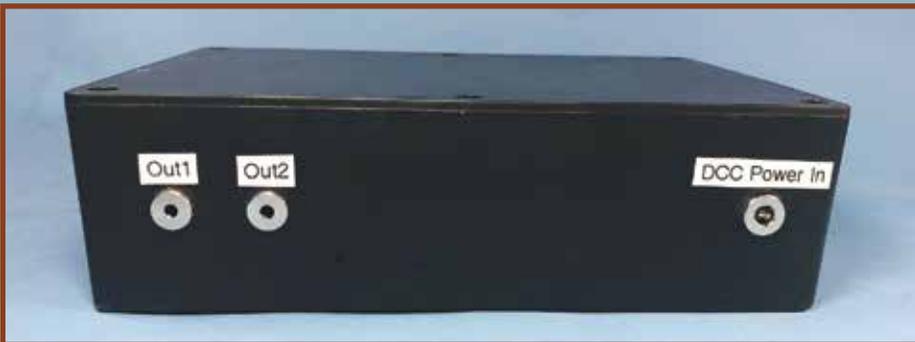
From left to right:

There is a 3.5mm input to plug in a basic DC power supply.

The rotary switch selects the three output options. The first option is power from the DC power pack input. The center option is motor power from the TCS T1. Finally, there is a straight DCC track power option.

The NCE faceplate is next. The NCE Power Cab needs to be plugged into the right plug for the system to work. The red LED will be on to indicate there are no shorts and the DCC Power Cab is working properly.

The USB port is for plugging in the cable supplied with the NCE USB board into my computer. This is used to program locomotives with JMRI Decoder Pro.

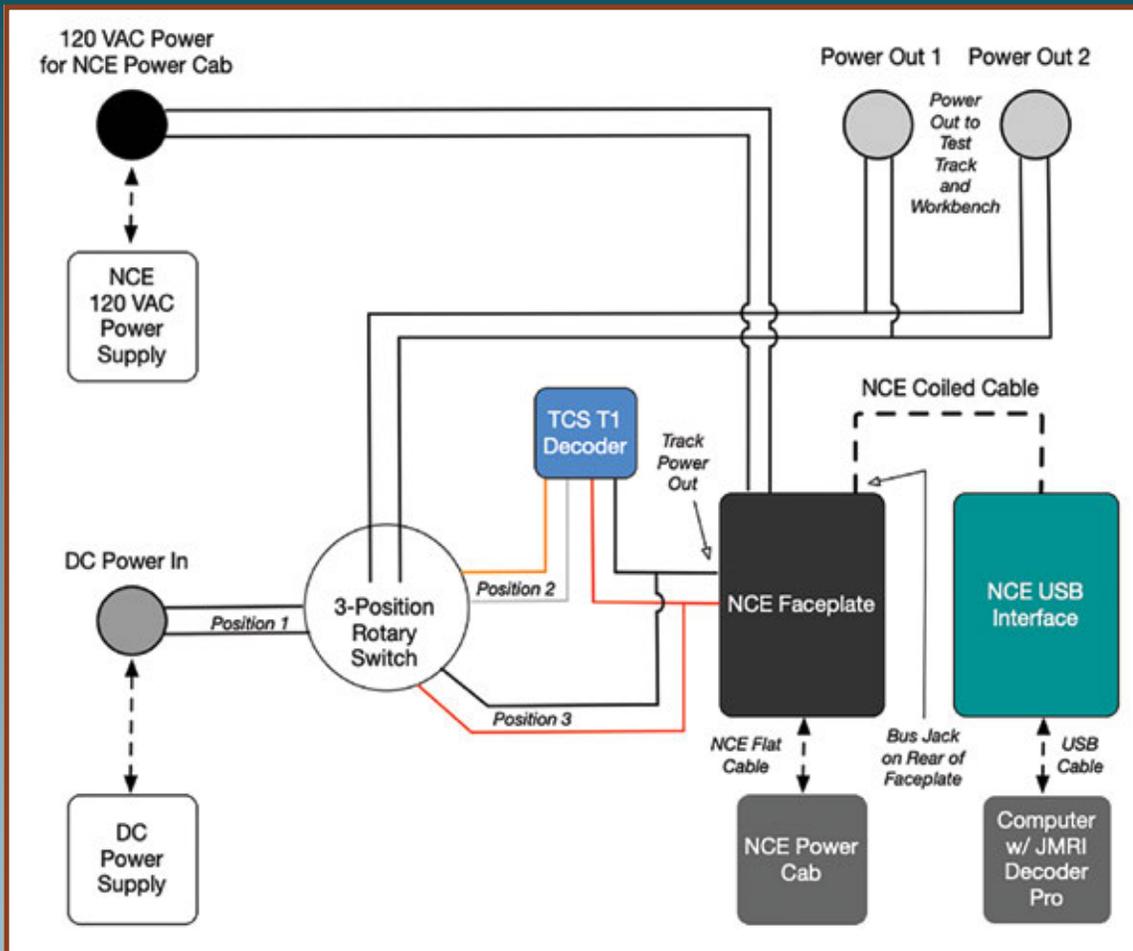


Above: The back of the junction box has the DCC 120 VAC Power In on the right. It is for plugging in the supplied AC power adapter that comes with the NCE Power Cab.

On the left, there are two identical 3.5mm outputs. One output will go to my test track, while the other can be used to plug in a second set of test leads for working on locomotives in a foam cradle.

Right: I test fit all the components in the box. The NCE USB board was mounted using stand-offs screwed to the bottom of the box.



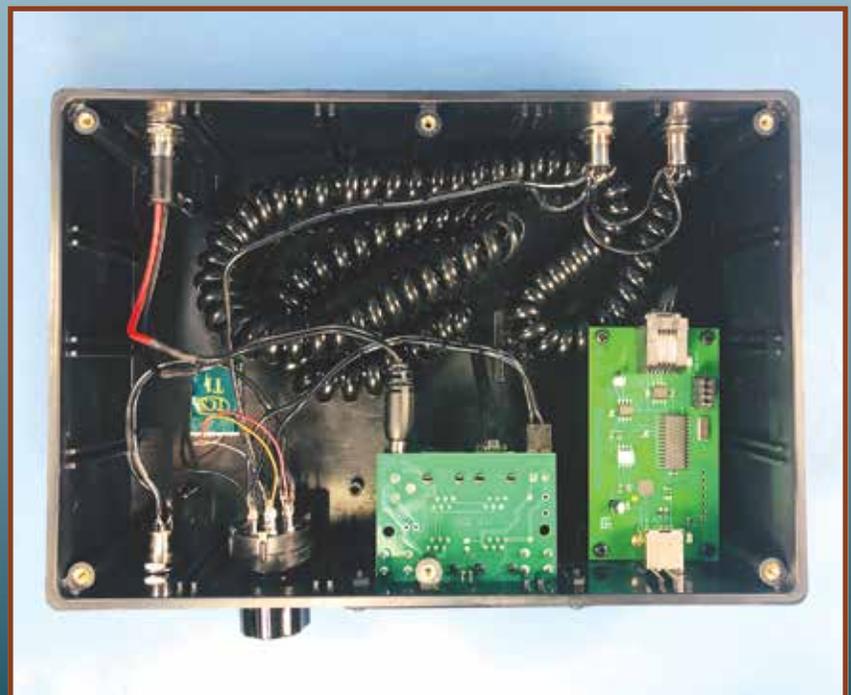


Left: Wiring diagram for the junction box.

Below right: This view shows the completed wiring inside the junction box. Looking from left to right across the back of the box (top of photo): The plug with the red/black wires is the power adaptor input for the NCE system and connects with the NCE faceplate. The two connectors on the right are 3.5mm female adaptors to create a pair of power outputs. These outputs connect to the center posts of the rotary switch which allow me to select the output from DC, the Decoder and straight DCC.

In the center/left is a TCS T1 Decoder. The black and red wires are connected to the track power outputs of the NCE faceplate. The orange and gray wires are connected as one input on the rotary switch. I programmed the Decoder to address "3333" since I'd never have a loco of that number, and then locked the Decoder from being accidentally reprogrammed. Selecting loco 3333 on the NCE throttle allows me to run a DC locomotive on the test track using the DCC throttle. I chose the TCS T1 Decoder because it has a high amperage output (1.3A) and should be fine with any HO or HO_n3 DC locomotive, including those with open frame motors. This setup has worked so well, that I may never need to use a DC power pack with my test track.

Looking from left to right across the front of the box (bottom of photo): There is a female plug used for a DC input from a DC power supply. This is wired directly to the rotary switch beside it. The rotary switch is used to control the power to the two output sockets. The options are DC power, DCC Decoder power or straight DCC track power. Next, the NCE faceplate takes its power from the power adaptor input from the back of the box, and sends a DCC signal to the Decoder and straight to the rotary switch. The last component is the NCE USB adaptor board. The USB board's supplied coiled wire is stuffed in the box and plugged into the back of the NCE faceplate.





Left: I didn't want to modify any of the NCE components and simply wanted to use them as the manufacturer designed them. A 120VAC power adapter is plugged into the NCE faceplate to provide power to the throttle and track. With the faceplate's connector inside the box, this connector became inaccessible. However, I found a female plug for the outside of the box that the NCE power adapter would plug into and soldered it to a wire with a male connector that would plug into the faceplate. This adaptor created a seamless transfer of power from the outside of the box to the faceplate without modifying any of the components. If I were to build this again, I'd add a toggle switch to the outside of the box and in-line with this wire so I could turn off the power without unplugging the NCE power adapter.



Above: Because there is a seemingly endless range of options for 3.5mm audio cables, I decided to set that as my standard for feed wires. I think that the options for coiled wires, different lengths and even retractable wires, will give me plenty of choices for customizing my setup in the future. These types of wires are readily available at almost any retail store. The downside of this choice is that these are a 3-conductor cable and I only need two conductors. The other downside is that one of the conductors is sometimes a braided shielding on the wire. That was the case for the wires I bought, so I simply combined the right and left stereo conductors to be treated as a single conductor and used the braided wire as the second conductor. The braided conductor with the wires I bought needed to be twisted into a wire and covered with heat shrink tubing. The wire in this photo is my power output used for a locomotive in a test cradle.

If you are in a larger scale, you will want to pick a different wire standard with larger gauge wire to handle higher current draws. This small wire has sufficient capacity for HO scale or smaller.



Left: This is the power input for my DC power supply. I added spade connectors so that this wire could be used on the screw terminals for any DC power supply.

Right: Laminating layers of 1/8-inch thick pine strips worked so well with my Telluride Extension (May/June 2014 GAZETTE), that I thought it would work well for creating a test track. I added a 24-inch radius curve since that's the minimum on my HOn3 layout. In this photo I'm gluing the strips together. Luckily my dad has a fully equipped wood shop and LOTS of clamps! The curved piece of wood on the workbench is a tool I made for checking track radiuses while I was building my layout.



Left: The wood working portion of my test track is completed here. I've added Masonite guard rails along the back as well as pine brackets for hanging the track from the underside of my cabinets. Stop blocks were added at the ends of the track too. Eventually, I ended up painting the test track white.

Right: I painted the test track white so that it would be visually appealing and make it easier to see. I added HO cork for roadbed and Micro Engineering HO/HOn3 code 55 FlexTrak. Eventually, I'll add foam cushions to the front of the stop blocks to protect the pilots of runaway locomotives.



Below: I used screws to attach the test track under my cabinets. In this photo, I have an HO Canadian Pacific 0-8-0 as well as an HOn3 D&RGW 2-8-0 demonstrating the flexibility of this dual gauge track system.



Right: To program locomotives I simply use the USB port to plug my computer in. On my computer I'm running JMRI Decoder Pro and can see the immediate results of changing the Decoder settings by testing the locomotive on the test track. The NCE Power Cab and my custom junction box provide all the intermediate power and connections. 🚂



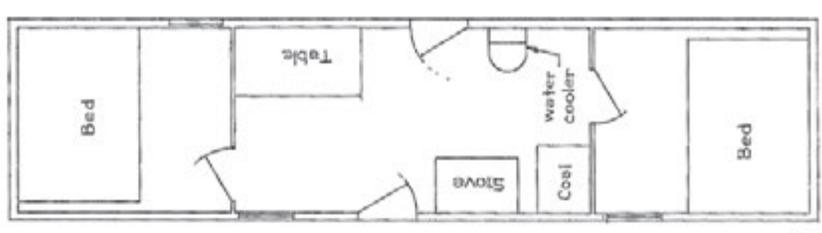
DENVER & RIO GRANDE WESTERN WATER SERVICE OUTFIT CAR 04432

DRAWN BY HERMAN H. DARR 24 July 1981©
SCALE: 3/16 INCH = 1 FOOT

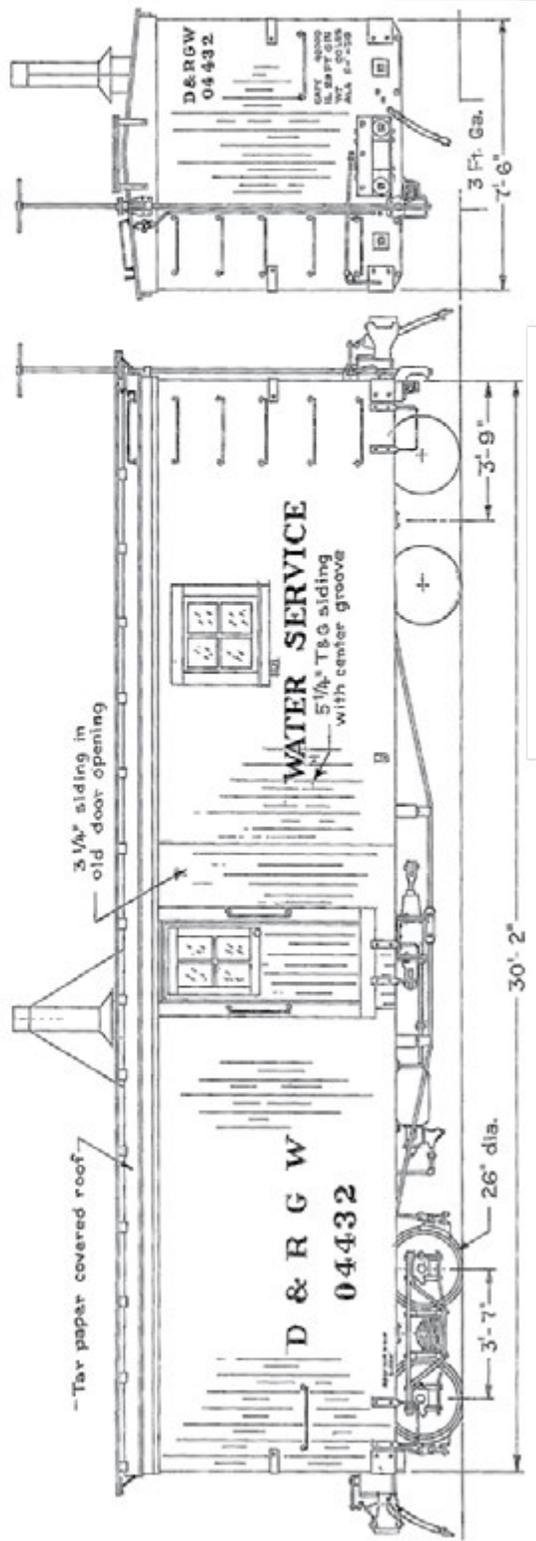


Car used by water service dept. as living quarters along with supply car 04904. Paint: box car red, white lettering. Data from photos, Jerry Hoffer and John Maxwell.

INTERIOR
NOT TO SCALE



Details not shown same as other side





by Charlie Getz

Photos by the author

TERMINAL THOUGHTS PART 2: MODIFYING THE WALTHERS MILWAUKEE STATION

In the January/February 2022 *GAZETTE*, I discussed the concept of creating a major urban station as the terminal for a free-lance narrow gauge line and interchange point with a standard gauge railroad. I mentioned the possible use of the HO scale Walthers' Everett Street Station and Train Shed kit to serve that purpose. In this column I will describe the planning and process I used to modify that kit and make it less recognizable as a particular prototype. The techniques described and modifications made need not be copied but can also be applied to other kits. I thank Walthers not only for the kit, but for the photo of the stock model, used here.

The Walthers kit is huge, packed in the largest kit box I have ever seen. Impressively designed, tackling modifications is not for the faint of heart. Actually, I very much like the design as presented incorporating all of the fancy Victorian era brick features of the prototype. The ornate chimneys alone are outstanding. Initially, I gravitated toward downsizing the model to loosely follow a prototype Union Station in Sterling, Colorado. Sterling is located on the plains east of Denver, served by both the Union Pacific and the Chicago, Burlington & Quincy

railroads. Though not served by narrow gauge, Sterling's compact yard, interesting mix of industries and stellar engine facilities make it a natural for modeling. SS Ltd. offers a series of HO kits based on Sterling's engine facilities including its distinctive roundhouse. Sundance Publications' out of print *Sterling Colorado: Crossroads of the Prairie* by James Jones and Russ Collman, 2000, is an excellent source of information on this fascinating place. Sterling's Union Station

servicing both the UP and CB&Q, is an impressive brick structure, shown here in a recent photo courtesy of History Colorado. However, while it would be feasible to create a fair representation of the depot using the Walthers kit, much of that kit would be discarded. For that reason, I rejected that possibility. Just hated to lose those chimneys!

Then I received the February 2021 *NMRA Magazine*, with a writeup of new MMR #665, Brook Qualman. She



A stock Walthers Everett Street station. Photo courtesy Wm. K. Walthers, Inc.

included a photo of herself pointing proudly to her major rebuild of the same Walthers kit she had used to model the Potter Street station in Saginaw, Michigan. Equally impressive, Potter Street was actually larger than the Everett Street station. Thus, she not only created an even larger station but, paraphrasing Ms. Qualman, not using a single piece from the Walthers kit as it came “out of the

box.” Though drawn to that modification, I felt making that many changes were too ambitious and that such a massive rebuild would take months.

I decided instead to make some simple adjustments by moving the distinctive tower toward the street and raising the trackside center section to match the end gables. Also, installing large skylights and other roof details to the flat roof sections

and new canopies to the front and trackside walls. These changes may not completely disguise the station’s heritage, but they do alter its appearance. Normally, I strongly recommend sketching any modifications but, in this case, I decided to modify on the fly as assembly proceeded. Were I modifying the kit as much as Brook Qualman did, I would draw a plan to document all of the needed changes. Here only a few changes were made, although some proved complicated.

The biggest change was extending the tower forward. As it comes, the kit’s tower is part of the main roof with minimal protrusion on the street side. By extending it forward, the impressive tower was accentuated becoming more of a focal point. It reminds me of downtown Denver’s Daniels & Fisher tower in the LoDo district that remains a local landmark. The D&F tower was once part of a major department store of the same name, long-since razed. The tower however remains. The modifications to accomplish this change were fairly straightforward re-



Above:
The Sterling,
Colorado, Union
station today.



Right:
The author's
modified
Walthers kit.



Above: This close-up shows the detail in the Walthers kit and the many modifications made by the author.

quiring modifying and adding new tower side walls and an extended foundation. I discovered that Holgate and Reynolds HO brick sheet #101 was a close match to the kit's brickwork and I cut new sub-side walls for the tower from styrene to fit into the small side walls provided. I preserved the original angled trim, designed to fit the roof, as an architectural feature with the new walls grafted on. I added a few SS Ltd. window castings to these new walls, plus three unexpected extra windows provided in the kit. Serendipity as for some unknown reason, the kit included an extra pair of matching larger windows and one

medium size. These extra windows fit the new tower walls perfectly, making it seem part of the original design. The SS Ltd. arched upper windows are consistent with the architecture and fit the space. Extending the foundation was as simple as cutting the existing foundation and pulling it forward to support the new tower, then splicing some matching styrene shapes in the newly created gap, hidden from view.

I also found (serendipity #2) that Delta Ceramcoat's craft paint Forest Green color exactly matched the Walthers' cast window/door plastic color. Those castings were simply Dullcoted and the SS Ltd.

windows were painted, thereby matching the Walthers' plastic ones. I treated the brick with a craft paint Heritage Brick color, followed by Robert's Mortar and used the Walthers' kit cover picture as a guide for the overall paint scheme. Wood trim was painted maroon, and the stone features, a craft paint sandstone color. I also opened the upper tower clock face insets to allow them to be lit from behind using thin styrene as a backing. The kit is designed to allow modification of the tower to either its original tall configuration or the shorter version of the 1950s. If modeling the shorter version, extra inserts

are provided to convert the open porches of the lower tower base into clock faces. I discovered these inserts were a perfect fit for my standard gauge line's herald. My proposed HO line is the Buffalo Creek Railway (not to be confused with New York's Buffalo Creek Railroad) but using the same distinctive round herald featuring a flour sack. For a Colorado line where agriculture covers half the state, this was a perfect herald as is the western-sounding name. (The New York short line was named Buffalo Creek due to its location near Buffalo, New York.) Well, that herald fit not only perfectly into the extra clock face inserts provided for the short tower option, but added a distinctive look to the tower. Again, serendipity.

Before tackling the roof modifications, I elected to add interior graphics and lights. To do this kit justice, given the large windows, a full interior should be added. This would be a major scratch-building project in its own right. If I had another few months of time, I would add the terrazzo tile floors, ticket and lunch counters, benches and chandeliers the interior would warrant. Not to mention the dozens of figures to represent passengers. One could literally spend more effort on an interior than on the kit itself. But since I am not sure how visible this station will be when placed on a future layout and facing deadlines, I elected to cheat by using graphics instead. The internet is marvelous for finding images of terminal station interiors in color and scaling them to whatever size is needed. My wife, Margaret, helped size, edit and print a perfect interior with tile floors and large benches. We essentially used this same two-story interior picture, cropped and edited to logically fit each window section while maintaining a uniform interior look. The graphics were curved from the window base to an interior scratched post/beam support system to create the illusion of a large space with tiled floor. Back lit, it is fairly effective. Placed in a layout setting with many models competing for attention, such an interior can pass muster whereas it would not be as successful in a focused examination of a single model. LED Christmas lights were installed from below as most model lights are frankly too wimpy for such a large space. This meant painting the interior of the walls black to prevent light bleed and plugging any light leaks. I also used sheets of dark construction paper for a ceiling as a light block for the unlit upper stories.

I raised the gabled center section on the rear center wall as it was not as tall

as the gabled annexes on each end which featured peaked roofs. To accomplish this, I separated the gable portion from the rest of the wall, added a few brick courses to the top of that wall to raise its height and recombined the pieces. The kit's angled gable roof section was used but modified for the new height by adding new side walls to convert the middle rear section from a gable to a wing. Again, this was accomplished with styrene sub-walls and brick overlay. I angled the new raised center flat roof to slope back to the tower base on one end, adding brick trim overlays on the side for interest. A new gutter and downspouts disguised the new tower base wall joints where they met the new center walls. This all required a bit of head scratching but nothing difficult. I painted all of the shingled/gabled roofs a dark craft green with a compatible weathering powder overlay to improve the texture of the roofs. I added CC Crow gray-seamed tarpaper to the flat roofs and installed large skylights from my stash, along with vintage Suydam vents and a roof access covered staircase.

The front tower extension provided space for a new canopy on each side. I also noticed that the track side had no covering, so I added a canopy along that flat wall to protect passengers from the elements and add interest. These canopies were made from joined board and batten styrene sheets to represent seamed metal, painted to match the roofs. Brackets were fabricated from wood and styrene using home-made jigs. I also created sidewalks from styrene, placed around the station to disguise the base when added to a layout. I'll add a lawn and flower beds to the tower side front later.

Although this kit is well engineered with excellent instructions, it is no snap-together kit. It requires careful and attentive assembly. For example, on the track side, the large wall sections interlock but are difficult to glue evenly to create an even plane. I used a combination of styrene cement and ACC to hold them firmly. The chimneys are fussy, fitting only one way, but precisely. This is also true of the complex roof and gables. It helps to dry-fit each part before gluing. They all fit well, but take your time.

The changes I made were sufficient to make the Everett Street station look a little different from the stock kit, satisfying my phobia against using easily recognizable kits. Its heritage is still evident but perhaps, just modified enough to appear distinct. The kit also includes two train sheds which when joined are almost 4-feet in length, shown in the Walthers'

photo. I will build those later when I am ready to install the station. The result is a very impressive terminal station complex that makes a statement. Adding three-rail track will allow narrow gauge varnish to use the station alongside standard gauge trains. And as I have hopefully demonstrated, this station is subject to any number of kit-bashing possibilities that can truly transform it into your own concept of a Terminal Station. You could easily do the same to a number of available kits. I hope I have shown that with some effort, you too can have an impressive Union Station terminal for your layout. I look forward to placing it on a layout one day as a centerpiece of my big city.

Well, that's all for now. Until next time, write, if the mood strikes.

Charlie



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BOOK REVIEWS

Ron's Books, P.O. Box 714, Harrison, NY 10528, 914/967-7541, ronsbooks@aol.com, www.ronsbooks.com continues to sell reprints of prototype catalogs by Silver Lake Images, LLC, in their Manufacturer's Catalog Archive. Each catalog sells for \$35.00.

General Motors Electro-Motive Division Volume 1 contains 122 pages of mainline and switcher Diesel locomotives in the early days of the company. It has chapters describing the benefits of Diesel locomotives and introduces the SD7 six-motor switcher and Electro-Motive's F3 Diesel. It also describes the Train of Tomorrow and tries to convince America's railroads that passenger traffic is profitable. While not a lot for the narrow gauge modeler, I found the description of early day Diesels and promotion of their use interesting.

ALCO Locomotives Volume 1 has 102 pages of data on ALCO Diesel and steam locomotives. There are many details of Diesel interiors shown, and the steamers range from little 0-4-0s to giant 4-10-2s. Contents list Diesel Switching Locomotives, Montreal Locomotive, Freight Locomotives of The Delaware

& Hudson, and Three Cylinder Locomotives.

Industrial Brownhoist Corporation Volume 1: Locomotive Cranes. This catalog has 108 pages of data and photos of cranes at work. There are cranes pile driving, loading, and unloading material; digging the earth; lifting a boat, wagons, and trucks; and clearing up wrecks. The book is full of parts illustrations perfect for those of you who want to build a locomotive crane. Great inspirations.

Bucyrus-Erie Company Volume 2. This catalog has 76 pages of draglines, cranes, and loaders. It also has a history of Bucyrus and the excavating industry titled Shaping the Earth for Better Living. Again, mostly large excavating machines, but with a smattering of little steam shovels and cranes worth modeling. The industrial excavation scenes are also worth looking at, some would make layouts on their own.

All four of these catalogs are printed on high quality paper with excellent photo and drawings reproductions. Keep checking the lists of these catalogs for something you may want to model.
Bob Brown.

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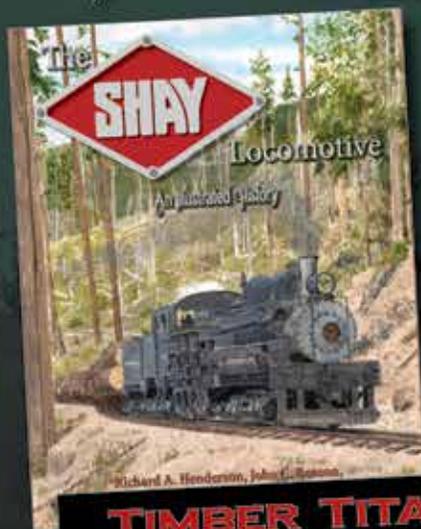
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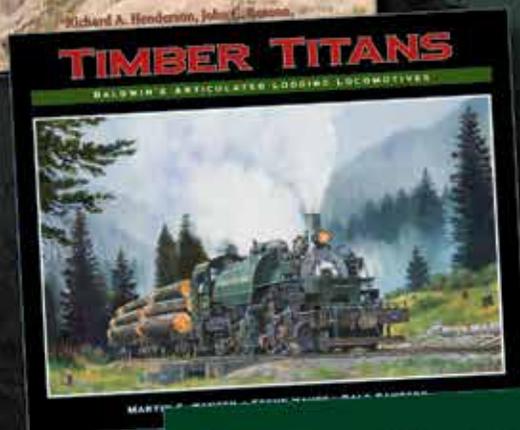


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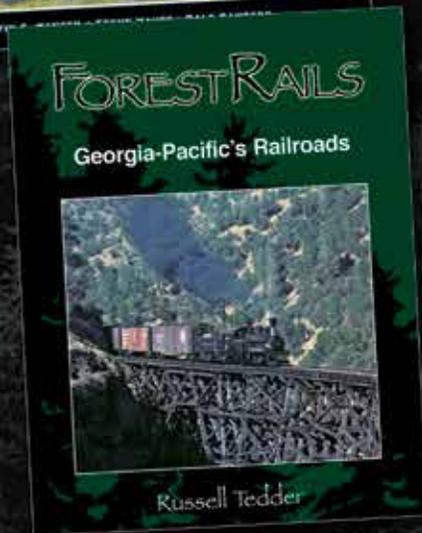


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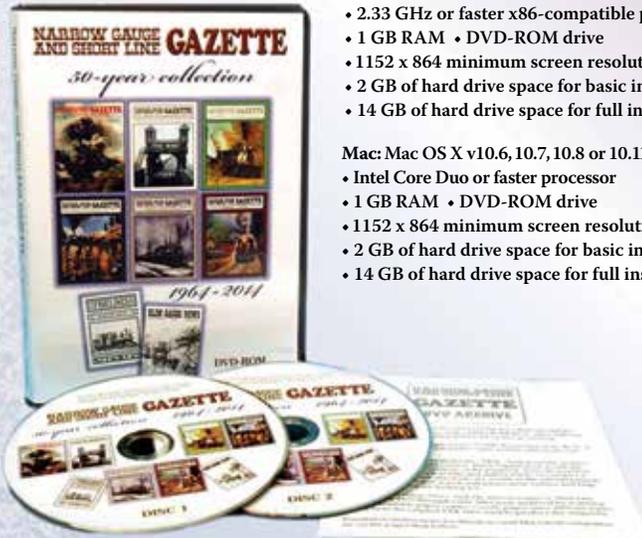
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DVD SET



SYSTEM REQUIREMENTS

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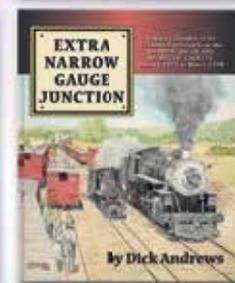
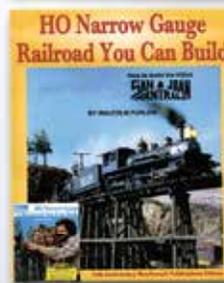
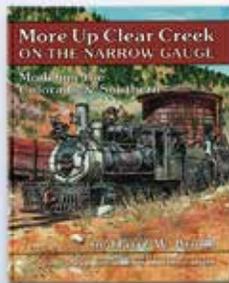
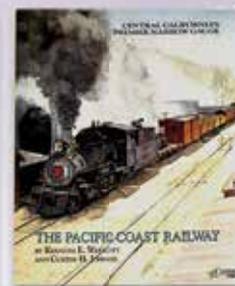
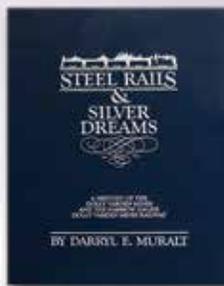
- 2.33 GHz or faster x86-compatible processor or Intel Atom 1.6 GHz or faster processor for Netbooks
- 1 GB RAM • DVD-ROM drive
- 1152 x 864 minimum screen resolution (1280 x 720 recommended)
- 2 GB of hard drive space for basic install
- 14 GB of hard drive space for full install

Mac: Mac OS X v10.6, 10.7, 10.8 or 10.11. Does not run on 10.15.1.*

- Intel Core Duo or faster processor
- 1 GB RAM • DVD-ROM drive
- 1152 x 864 minimum screen resolution (1280 x 720 recommended)
- 2 GB of hard drive space for basic install
- 14 GB of hard drive space for full install.

*** ATTENTION APPLE MAC USERS:**
The latest release of the Mac Operating System, OS 10.15.1 Catalina, is not compatible with the Gazette DVD Archive. Apple no longer supports 32-bit applications. There are no plans to issue a 64-bit version of the Archive, so don't order it unless you have a Mac running an older OS. The Archive still works fine on Windows PC operating systems.

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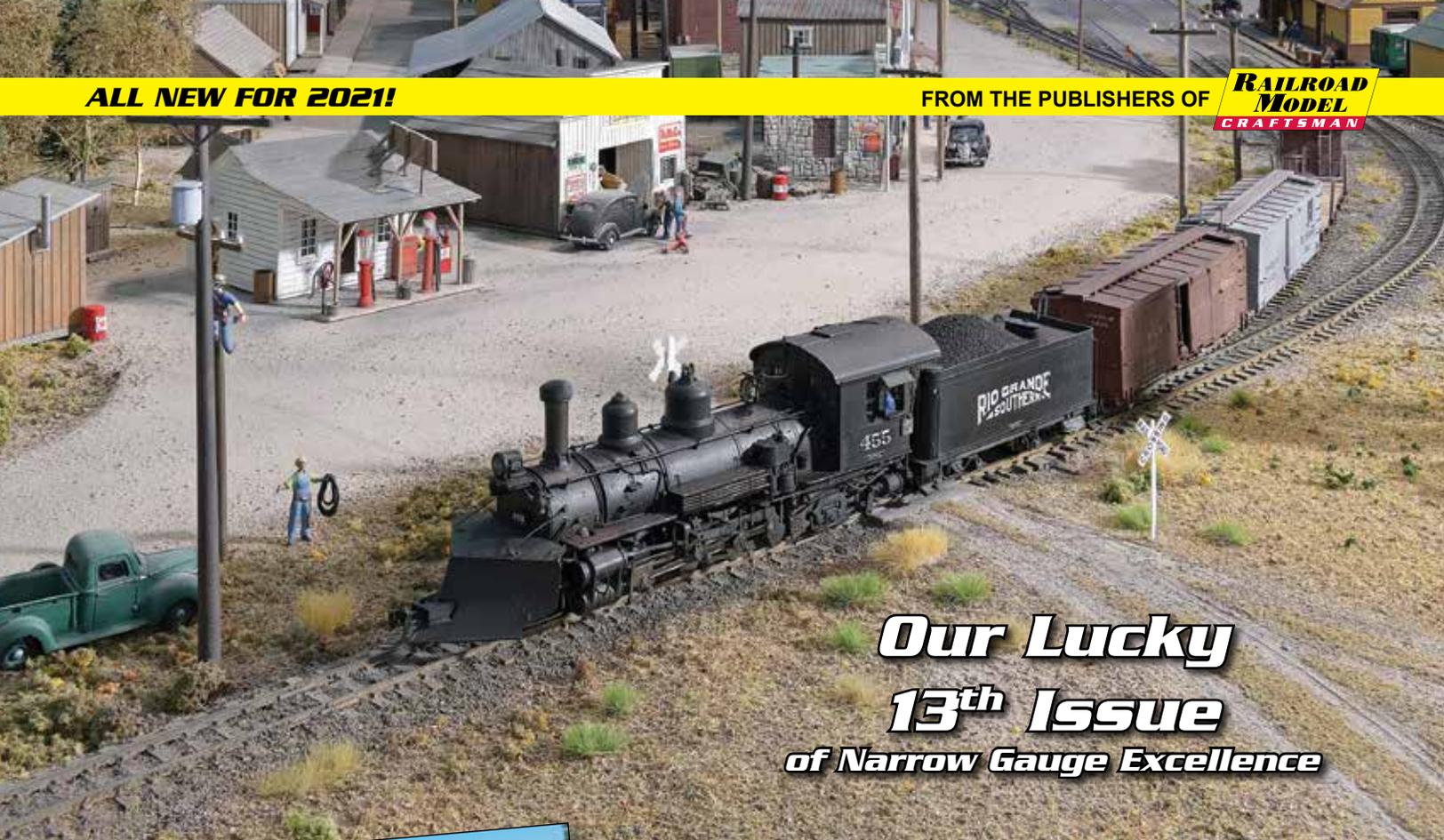
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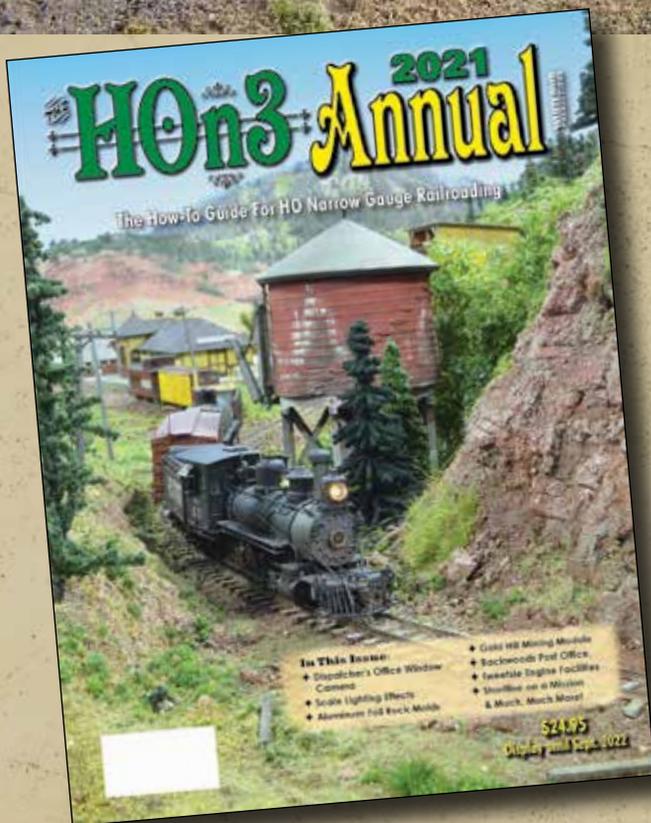
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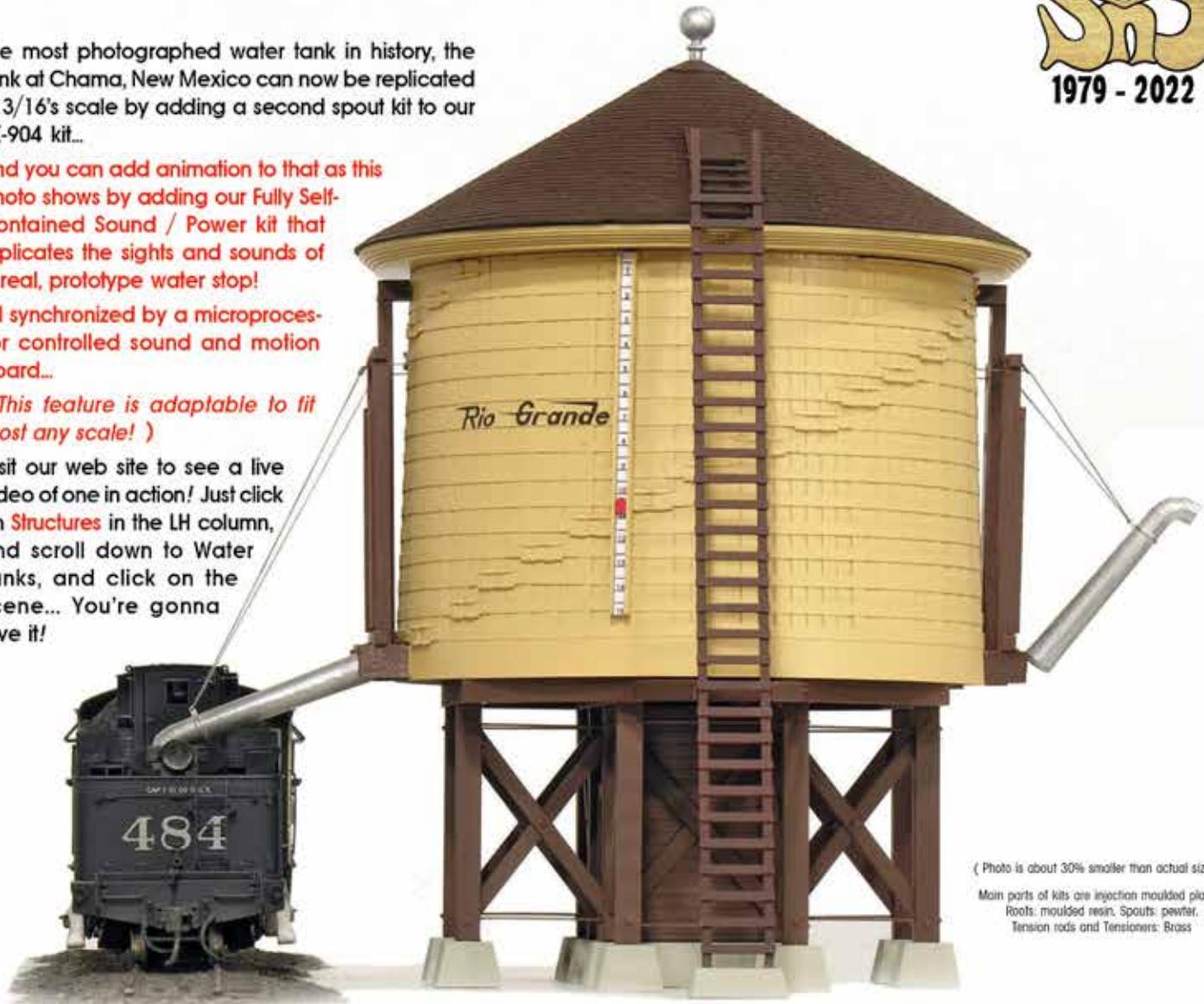
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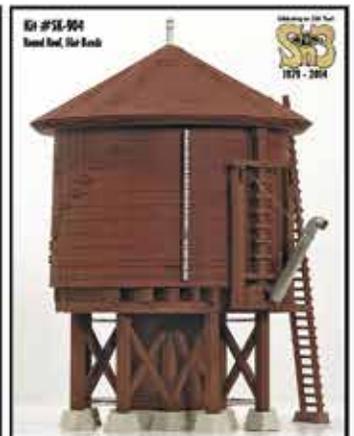
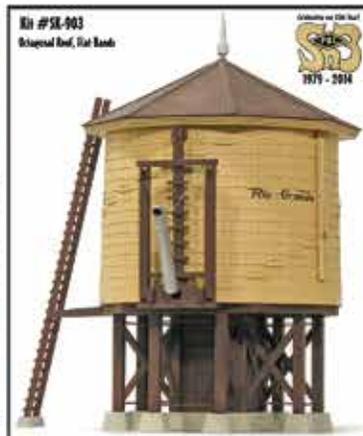
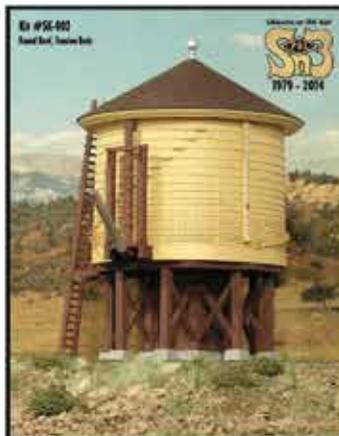
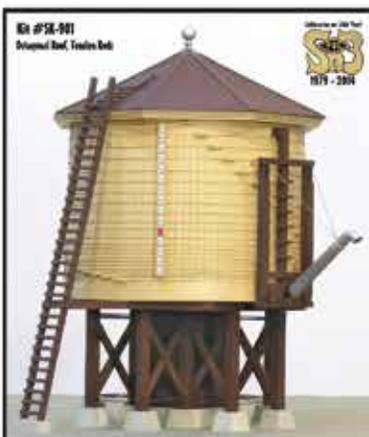
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